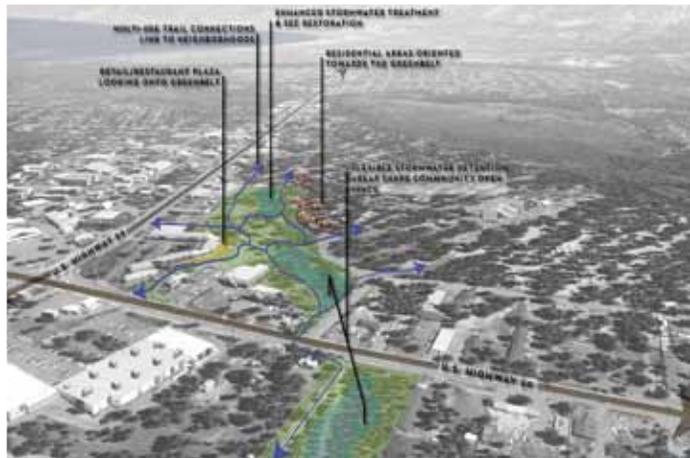




Tahoe Valley Area Plan

Linking Neighborhoods ❖ Building Community ❖ Promoting Recreation

June 4, 2014



**Public
Review
Draft**

TAHOE VALLEY AREA PLAN

City of South Lake Tahoe

Public Review Draft, June 4, 2014

Adopted by South Lake City Council on [DATE]

Adopted by Tahoe Regional Planning Agency on [DATE]

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Strategic Growth Council

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SECTION 1

Introduction



The Tahoe Valley Area Plan (Area Plan) serves as the comprehensive land use and zoning plan for the Tahoe Valley area, consistent with the Lake Tahoe Regional Plan (Regional Plan) and the City of South Lake Tahoe General Plan (General Plan). The plan is intended to realize the community's vision, assist in achieving and maintaining TRPA's Environmental Threshold Carrying Capacities, implement the City of South Lake Tahoe Sustainability Plan, implement the Tahoe Metropolitan Planning Organization's (TMPO) Sustainable Communities Strategy, and implement the policy direction of both the Regional Plan and General Plan.

The Tahoe Valley Area Plan defines a vision for the future of the Tahoe Valley area. The Tahoe Valley Area Plan planning area (plan area) is a 337-acre area, centered on the US 50 and SR 89 "Y" intersection, in the southwest corner of South Lake Tahoe. The plan area is roughly bounded by 10th Street on the north, E Street on the south, Truckee Drive on the east, and Julie Lane on the west (Figure 1). Except for two parcels at the southern end along US 50, the Area Plan includes the same territory as the Tahoe Regional Planning Agency's "Plan Area Statement #110" (located on TRPA maps G-18, G-19, F-18, and F-19). In addition, a portion of the Area Plan's boundaries aligns with TRPA's Town Center Overlay District.

The Tahoe Valley area developed primarily in the 1950s as a commercial corridor for highway travelers. While some of South Lake Tahoe (such as the Stateline/Ski Run area) has been transformed in the last ten years, the Tahoe Valley area has remained relatively stagnant. Exceptions include the Staples, Longs, Pier One, TJ Max, and Coldwell Banker where significant improvements as a result of private investment have taken place.

This Area Plan sets a vision for the Tahoe Valley area based on a realistic assessment of the area's potential. The Area Plan sets policies as well as implementation measures to ameliorate existing conditions and create new development and redevelopment opportunities. It describes how this area can focus the resources needed to become a vital economic center of the south shore and the Tahoe Basin while reflecting Tahoe Valley's unique characteristics and supporting the local community's vision.

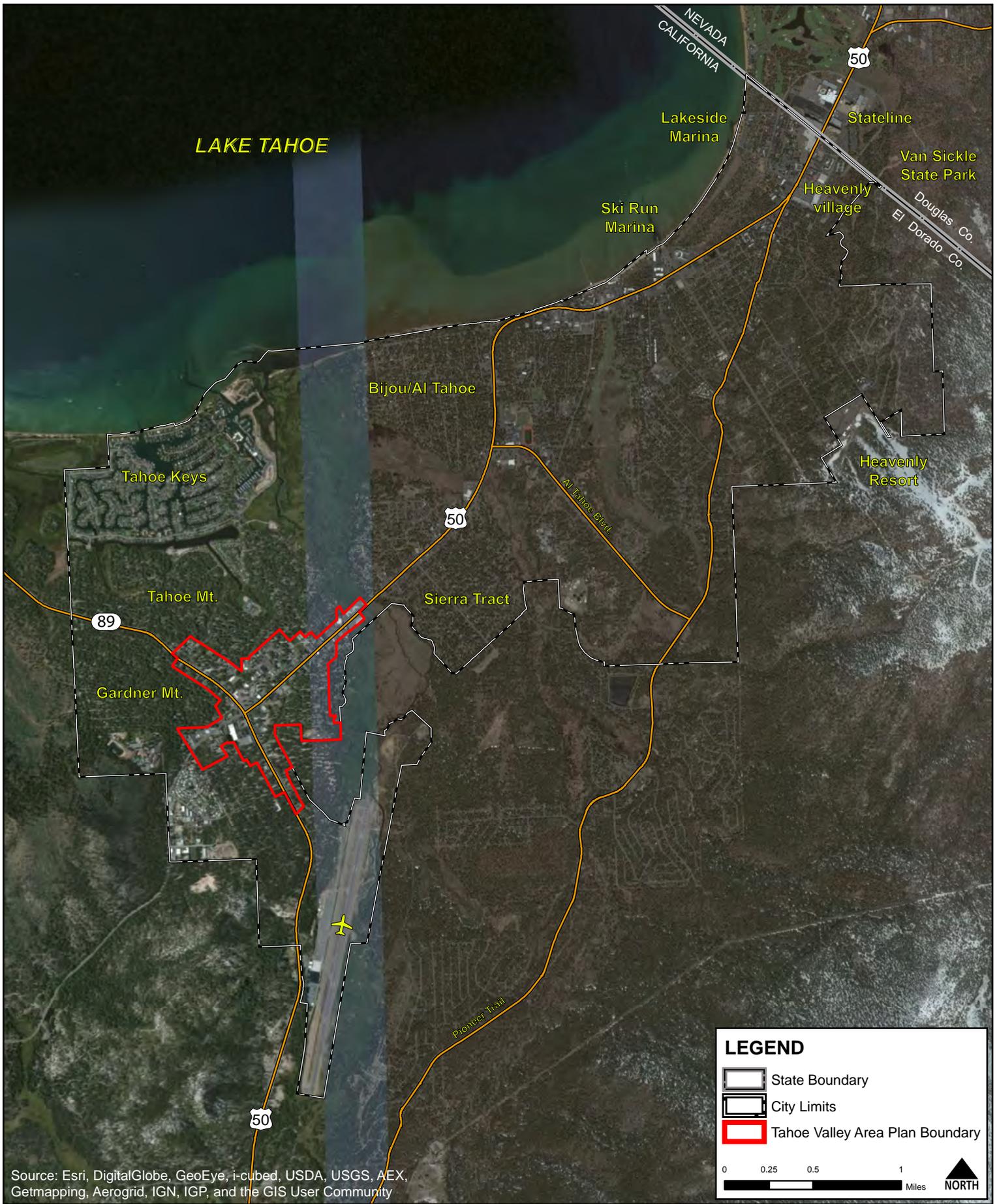
The Area Plan provides more detailed direction for the Tahoe Valley area and incorporates both the Regional Plan and the General Plan by reference, including the respective implementing ordinances. The Area Plan integrates these plans to the extent practical in order to simplify compliance with local and regional requirements, and improve the efficiency of plan administration. It establishes the land use regulations, development standards, implementation strategies, and needed environmental improvements for the area, and encourages new development and redevelopment that enhances the built environment in order to revitalize the Tahoe Valley community. Renovation of this gateway to the south shore is important to achieving the City's economic development goals for revitalizing the community, improving infrastructure, and diversifying the local tax base.

1.1 Relationship to the Tahoe Valley Community Plan

Tahoe Valley was initially designated a “Preliminary Community Plan” area in the City’s 1999 General Plan and TRPA’s 1987 Regional Plan. The Tahoe Valley Community Plan was initiated in February 2003 through a joint planning effort by the City and the TRPA. That process resulted in the Draft Tahoe Valley Community Plan that was published in March 27, 2007, and presented to the City Council on April 3, 2007. The City retained Pacific Municipal Consultants to prepare an Environmental Impact Report (EIR) on the draft community plan, which was completed in November 2008 but was not circulated. Because TRPA was in the midst of updating their 1987 Regional Plan that had the potential to alter land use regulations and incentives strategies, a decision was made to postpone adoption of the community plan until after an updated Regional Plan is adopted. Subsequently, TRPA adopted an updated Regional Plan in December 2012 that fundamentally changed the community planning process and provided an alternative planning process for local jurisdictions to adopt conforming Area Plans.

1.2 Area Plan Adoption

The South Lake Tahoe City Council and the Tahoe Regional Planning Agency Governing Board will hold public hearings and take action on the Tahoe Valley Area Plan. Once found in conformance with the City’s General Plan and TRPA’s Regional Plan and adopted by both, this Area Plan will serve as the governing plan for the Tahoe Valley area for both the City of South Lake Tahoe and TRPA. This Area Plan will supersede Plan Area Statement #110 for the purposes of land use regulation for both agencies and will provide management direction for all projects proposed within the plan’s boundaries.



1.3 Organization of the Area Plan

Following this Introduction, Section 2 characterizes the legal authority and regulatory setting of South Lake Tahoe. Section 3 provides an overview of the Area Plan's existing conditions and opportunities and constraints for redevelopment and conservation. Section 4 summarizes the plan's vision for the future and guiding principles for development and redevelopment. Sections 5-12 outline the policy direction for the plan, with individual sections on the following:

Section 5 – Land use and Community Design

This section supplements the Land Use Element of the TRPA Goals and Policies, and the Land Use Element of the City of South Lake Tahoe General Plan. This Section includes the Area Plan's Goals and Policies related to land use and community design, and establishes land use and zoning regulations for the Area Plan.

Section 6 – Economic Development

This section supplements the Economic Development Element of the City of South Lake Tahoe General Plan. This Section includes the Area Plan's Goals and Policies related to economic development and sustainability to create a vibrant and sustainable local economy.

Section 7 – Housing

This section supplements the Housing Element of the TRPA Goals and Policies, and the Housing Element of the City of South Lake Tahoe General Plan. This Section includes the Area Plan's Goals and Policies related to housing to achieve the Plan's vision of increasing the availability of quality workforce housing and affordable housing.

Section 8 – Transportation and Circulation

The Transportation and Circulation Section is a supplement to the TRPA Regional Plan, TRPA/TMPO Regional Transportation Plan and to the Transportation and Circulation Element of the City of South Lake Tahoe General Plan. Consistent with these plans, this chapter lists the specific policies applicable to the Area Plan and describes the improvements necessary to implement these policies.

Section 9 – Public/Quasi-Public Facilities and Services

The Public/Quasi-Public Facilities and Services Section is a supplement to the Public Service Element of the TRPA Goals and Policies Plan and the Public/Quasi-Public Facilities and Services Element of the City of South Lake Tahoe General Plan.

Section 10 – Natural and Cultural Resources

The Natural and Cultural Resources Section is a supplement to the Conservation Element of the TRPA Regional Plan and the Natural and Cultural Resources Element of the City of South Lake Tahoe General Plan. It contains the Area Plan's environmental conservation and management strategy. It lists the needed environmental improvements for the Area Plan.

Section 11 – Recreation

The Recreation Element is a supplement to the Recreation Element of the TRPA Regional Plan and the City of South Lake Tahoe General Plan. It contains the plan's strategy for improving recreational opportunities and identifies proposed recreational improvements within the plan.

Section 12 – Health, Safety, and Noise

This section supplements the Noise Element of the TRPA Goals and Policies, and the Health and Safety Element of the City of South Lake Tahoe General Plan. This Section includes the Area Plan’s Goals and Policies related to public health and safety issues.

Section 13 – Implementation

The Implementation Element is a supplement to the Implementation Element of the TRPA Goals and Policies Plan and the Implementation Program of the City of South Lake Tahoe General Plan. It describes public and private sector implementation strategies including capital improvement projects and planning and coordination activities, including a summary of incentives, and the plan reporting and revision process.

Appendix A – References

Appendix B – TRPA Height Findings

Appendix B includes height findings that are required for any structures that exceed three stories in height.

Appendix C – Development and Design Standards

Appendix C sets development standards, design standards and permissible uses for the Area Plan’s land use districts. The standards are intended to improve the scenic quality of the built environment, promote a sense of place, and support the community’s desired community character. Development, redevelopment, or expansions of existing uses will be reviewed by the City or TRPA for consistency with these standards.

Appendix D – City of South Lake Tahoe Green Building Program

Appendix D sets criteria for earning City incentives for incorporating exceptional energy efficiency in new and redeveloped projects.

SECTION 2

Legal Authority and Regulatory Setting

The City of South Lake Tahoe plans and regulates within the policy and regulatory framework of both the Tahoe Regional Planning Agency (TRPA) and the State of California. This section describes the policy and regulatory framework for the Tahoe Valley Area Plan.

The purpose of the Tahoe Valley Area Plan is to define land use regulations and guidelines for planning decisions. The Tahoe Valley Area Plan presents principles, goals, policies and implementation strategies designed to encourage redevelopment, create a vibrant walkable pedestrian oriented community and provide for environmental improvements. The Area Plan is used by the City and TRPA to review specific development proposals in Tahoe Valley. The Plan also provides direction to property owners, community groups, and interested individuals in planning and design of development and redevelopment projects.

The Tahoe Valley Area Plan, once adopted, will become a part of TRPA's Regional Plan and the City of South Lake Tahoe General Plan. The Area Plan will also revise the official zoning map of City of South Lake Tahoe.

2.1 Tahoe Regional Planning Agency

Regional Plan

In December 2012, the TRPA Governing Board adopted an updated Regional Plan. The TRPA Regional Plan Goals and Policies serve as a guide for all future land use decisions within the Lake Tahoe Region. Priorities of the updated Regional Plan that apply to this Area Plan include:

- Accelerating water quality restoration and other Threshold gains by supporting environmental redevelopment opportunities, restoration of disturbed lands and Environmental Improvement Program (EIP) investments.
- Transitioning to more permitting by local governments to create “one-stop-shopping” for homeowner improvements in order to return TRPA to the more regional role the Compact originally intended.
- Creating walkable communities and increasing alternative transportation options.
- Retaining the established regional growth control system. Under this system, rampant overdevelopment was stopped and open spaces preserved. Most of the policies from the 1987 Regional Plan stayed in place with adoption of the 2012 Regional Plan.
- Creating a more efficient planning system that integrates TRPA requirements into the plans and permits of other government agencies.

- Encouraging property owners to transfer development rights from sensitive or outlying areas to Town Centers with the goal of restoring these lands.
- Eliminating regulatory barriers to the environmental redevelopment of rundown buildings.
- Simplifying burdensome regulations for homeowners while achieving threshold gain.
- Integrating with the TRPA Regional Transportation Plan to support sidewalk and bike trail projects that reduce automobile dependency and increase walkability and safety.
- Continuing to deliver restoration projects under the EIP that achieve erosion control on roadways and restore forests and wetlands.

TRPA’s Regional Plan Conceptual Land Use Map classifies Tahoe Valley as Mixed-Use and also designates a portion of the area as a Town Center Overlay District. TRPA’s Regional Conceptual Land Use Map is depicted in Figure 2.

The following description identifies the intention and purpose of TRPA’s land use classification, overlay districts and special designations:

Mixed-Use: Urban areas that have been designated to provide a mix of commercial, public service, light industrial, office, and residential uses to Region or have the potential to provide future commercial, public service, light industrial, office, and residential uses. The purpose of this classification is to concentrate higher intensity land uses for public convenience.

Town Centers: Areas that contain most of the regions non-residential services Town Centers are targeted for redevelopment in a manner that improves environmental conditions, creates a more sustainable and less auto-dependent development pattern and provides economic opportunities in the region. This district functions as a pedestrian- and transit oriented , mixed-use activity center that encourages mix of uses that promotes convenience, economic vitality, and a pleasant quality of life and improved access to a greater range of facilities and services for surrounding residential neighborhoods and visiting tourists.

Area Plan

Area Plans allow for planning that is responsive to the unique circumstances of communities within the Tahoe Region. TRPA and local governments, in close coordination with the local community, may prepare plans for a specific area in conformance with the Regional Plan. The authority and requirements for this Area Plan are provided in Chapter 13 (Area Plans) of the TRPA Code of Ordinances.

Sustainable Communities Strategies

California’s Senate Bill 375 (SB 375) requires metropolitan planning organizations to focus regional land use and transportation policies to reduce greenhouse gas (GHG) emissions from cars and light trucks in order to meet targets established by the California Air Resources Board’s Regional Targets Advisory Committee. SB 375 calls for each metropolitan planning organization to develop a Sustainable Communities Strategy (SCS) identifying the transportation, land use, and housing strategies that will reduce regional GHG emissions. The TRPA adopted Regional Transportation Plan serves as the Region’s Sustainable Community Strategy Plan.

The Tahoe Valley Area Plan is designed to be consistent with the TRPA Regional Transportation Plan and the SCS by providing a framework for implementing land use and transportation practices to reduce GHG emissions. These strategies include creating a mixed-use, pedestrian and transit oriented land use pattern, providing for alternative modes of transportation and encouraging a variety of housing types within close proximity to transit, services and employment centers.

2.2 State of California

The Tahoe Valley Area Plan is designed to meet State of California requirements for specific plans and to implement facilitate the implementation of development goals and policies by establishing zoning districts, standards and criteria for development, and set the distribution, location and extent of planned land uses consistent with the adopted City General Plan.

2.3 City of South Lake Tahoe

The City of South Lake Tahoe must adhere to both TRPA plans and regulations adopted pursuant to the Compact and to California State law, to the extent that State laws do not conflict with the Compact. Under the Compact, TRPA addresses general regional matters, while the City retains its power over purely local matters. The City can adopt more strict regulations than TRPA and can regulate matters over which TRPA has no authority.

General Plan (2011)

The City of South Lake Tahoe General Plan designation for Tahoe Valley is Town Center (TC). This designation provides for a mixture uses including tourist accommodation, commercial, intensive recreation, high-density residential, and mixed-use residential. This designation is applied to areas that are currently developed as commercial/visitor centers, have excess land coverage, where vertical mixed-use projects are appropriate, and are near commercial, employment, transit, and public services. The City's General Land Use Diagram is depicted in Figure 3.

The overall purpose of the Town Center is to:

- Provide for the orderly, well-planned, and balanced growth of the Town Center and support the area's role as an important commercial, tourist, and recreation center that provide full services to residents while appealing to visitors.
- Promote the Town Center as a pedestrian- and transit-oriented, mixed-use activity center and a gateway to outdoor tourism recreational destination and expand its role as an economic center for residents of South Lake Tahoe.
- Encourage a mix of uses that promotes convenience, economic vitality, and a pleasant quality of life and improve access to a greater range of facilities and services for residents and visitors.
- Establish development and design standards that improve the pedestrian- and transit-orientation and visually quality of development and create a unified, distinctive, and attractive community character.

The General Plan includes the following policies that are directly applicable to the Tahoe Valley Area Plan:

Policy LU-2.2 directs the City to periodically update and implement the four community plans, of which Tahoe Valley is one, as a way to focus development commodities and revitalization efforts.

Policy LU-2.7 directs the City to transform the Tahoe Valley area into an attractive gateway commercial district that serves both residents and visitors.

Policy LU-2.9 defines Tahoe Valley as one of the City's nodes that is targeted for future development and revitalization.

Policy LU-2.10 encourages the use of incentives to create a compact, mixed-use, transit-oriented, and higher-density development pattern within the Tahoe Valley area.

Policy LU-2.11 designates the Tahoe Valley as a primary area in the City for resident-serving commercial uses, workforce housing and affordable housing. Furthermore the policy encourage transforming the area into a contemporary, pedestrian-oriented, mixed use, commercial service district served by a transit center and alternate transportation opportunities.

Policy LU-2.13 designates the Tahoe Valley area as a social and economic center that includes a variety of housing types, mixed-use development, recreational or social gathering spaces, cultural uses, civic uses and facilities, and commercial services.

The development of the Tahoe Valley Area Plan is intended to meet the directive of the General Plan policies outlined above.

Sustainability Plan

The City of South Lake Tahoe adopted policies for sustainable development and "green" buildings in the Land Use Element of the General Plan. Policies call for creating a sustainable and energy-efficient built environment as a key vision of the City of South Lake Tahoe. The policies provide direction on the use of "green" building technology, energy efficiency, and strategies to achieve sustainable development practices.

Currently, the City has implemented specific strategies including providing streamlined permitting to encourage "green" building practices and instituted a plastic bag ban for grocery stores, food vendors, and retail establishments.

Plan Area Statements

The Tahoe Valley Area Plan completely encompasses the area currently within Plan Area Statement (PAS) 110, South "Y". Once adopted, the Tahoe Valley Area Plan will replace PAS 110.

Standards and Guidelines

The City of South Lake Tahoe and TRPA have jointly adopted Standards and Guidelines that include citywide design standards, guidelines, and specific design standards for the three previously adopted Community Plans.

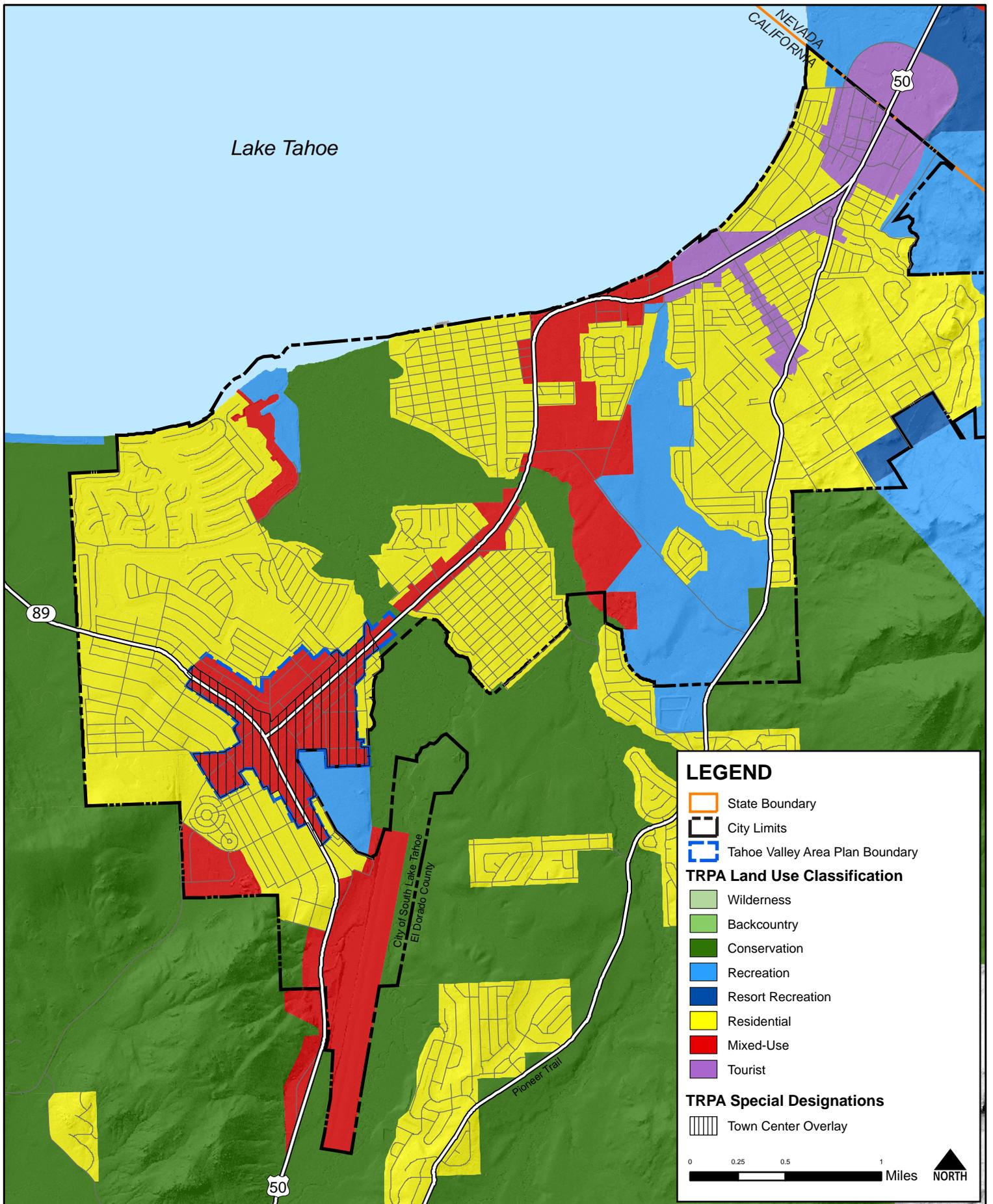
The citywide design standards and guidelines as well as specific substitute design standards tailored towards the Area Plan are incorporated in the Tahoe Valley Area Plan and will provide design guidance for the Area Plan.

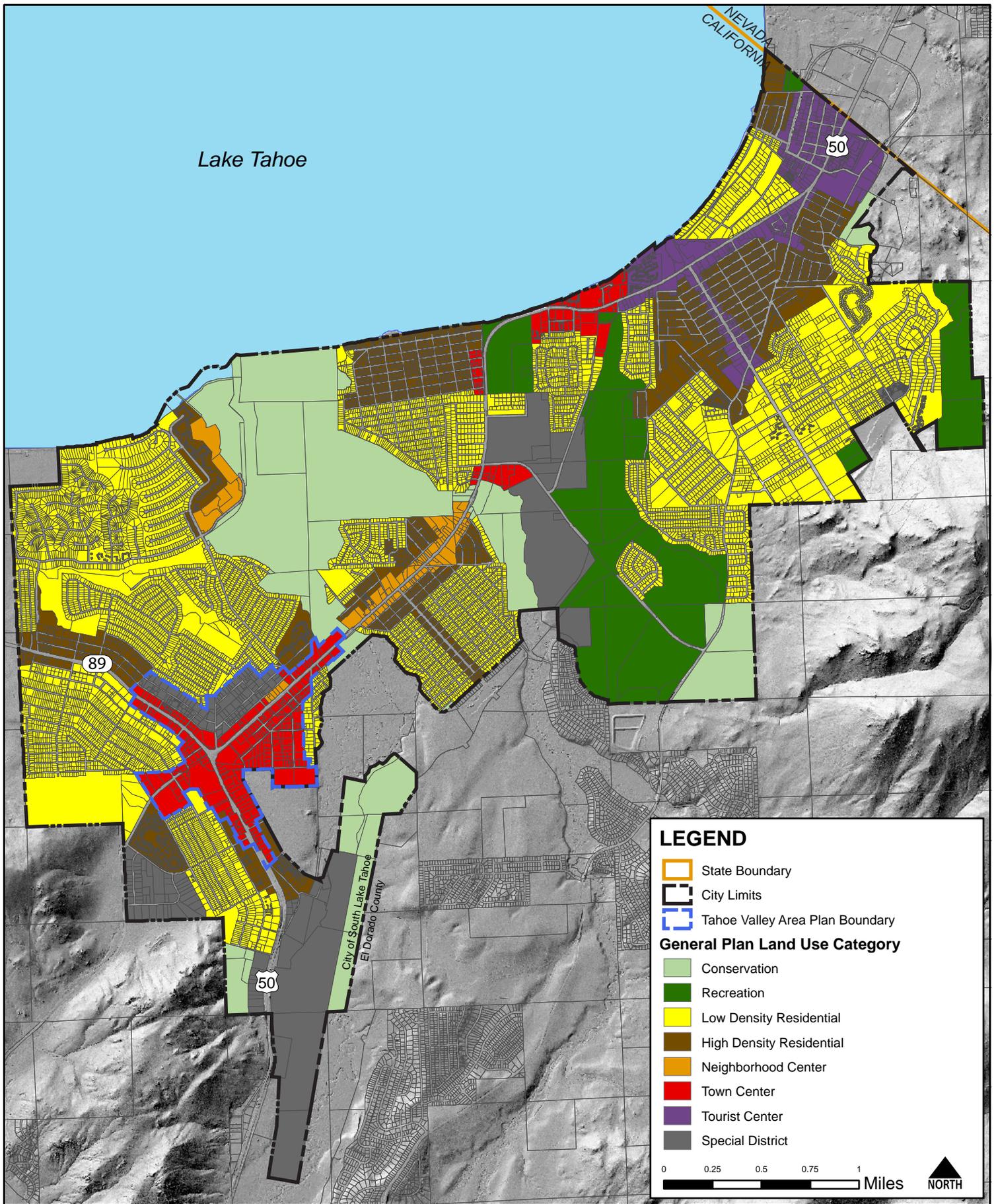
Smart Growth Principles

In August 2005, the South Lake Tahoe City Council endorsed a set of Smart Growth Principles for possible inclusion in the City General Plan and in the Regional Plan for South Lake Tahoe. These principles are organized under the following headings:

- Land Use
- Transportation
- Community Livability
- Governance

The Tahoe Valley Area Plan is designed to implement the City's smart growth principles by encouraging a land use pattern that focuses on walkable, compact mixed-use centers, with access to transportation choices while preserving open spaces and critical environmental areas.





SECTION 3

Existing Conditions



This section describes the existing physical conditions within the Tahoe Valley area, both natural and human-made. The Tahoe Valley area has been extensively altered by human activity and development since it was first settled in the late 1800s. The present pattern of development in the area has evolved primarily since the 1950s. The conditions summarized in this section act as opportunities or constraints (or sometimes both) to development, redevelopment, and conservation of resources within the plan area.

This section is organized into the following topics: Land Use, Economic Conditions, Transportation and Circulation, Public/Quasi-Public Facilities and Services, Natural Resources and Cultural Resources, Open Space, and Health and Safety.

3.1 Economic Conditions

The economy of South Lake Tahoe is largely dependent upon tourism, which fluctuates seasonally. The lodging industry at the South Shore has experienced substantial difficulties over the past 10 years. The number of annual rooms rented has declined 35 percent. More recently, a slight uptick has been observed in 2010/2011, and reports indicate the 2011/2012 also showed slight improvements. However, these figures are still substantially lower than the levels seen in 2001/2002 and before, which translates into less local revenue generated as well as fewer job opportunities. (TTD, 2013).

The Tahoe Valley area is largely dominated by strip commercial development along South Lake Tahoe's key corridor (US 50). Shortcomings of this type of development include the area's many curb cuts for driveways, relatively small and fragmented parcels, stand-alone structures, older motels struggling to be profitable, and lack of continuous sidewalks, bike paths, landscaping, and other amenities. In addition, this type of development pattern is difficult and costly to modify. South Lake Tahoe also has a strict regulatory environment created around the need to protect the environment in the Tahoe Basin, which adds to the challenges of redevelopment.

In summary, there is a clear need to revitalize the physical appearance and accessibility of the area to increase economic activity. The area is in a unique position to capture more local-resident-serving commercial sales, as well as capturing more tourist sales.

3.2 Land Use

Neighboring Land Uses

The Tahoe Valley plan area is bounded on the north, west, and south primarily by residential subdivisions, dating from the 1960s to the 1990s. The plan area is bordered on the east by the Upper Truckee Marsh and to the southeast by the South Lake Tahoe Airport.

Land Uses

The plan area is made up of a variety of existing land uses, including residential, commercial, tourist accommodation, industrial, and public service. Table 1 summarizes existing uses within the plan area by acreage, percentage of total acreage, and number of parcels. The plan area encompasses 336 acres and 482 individual parcels (not including existing roadway/infrastructure related parcels) as shown in Figure 4. Tahoe Valley is the gateway neighborhood for South Lake Tahoe, and includes various commercial uses such as clothing stores, factory outlets, automotive supply stores and dealerships, drug and liquor stores, restaurants, and supermarkets. These uses occupy structures ranging in age and physical condition, from the 1890's to present day construction.

Table 1 Existing Land Use		
Land Use	Acres	Percent of Acreage
Residential	37.2	11.1%
Tourist	12.0	3.6%
Commercial	141.0	41.9%
Public	19.2	5.7%
Quasi-Public	8.2	2.4%
Recreation	3.3	.01%
Vacant Private	18.1	5.4%
Vacant Public	14.8	4.4%
Open Space	20.2	6.0%
Roads and Infrastructure	62.5	18.6%
TOTAL	336.5	100%
Source: TRPA GIS, March 2014.		

Residential

Based on 2013 TRPA data, the plan area includes a total of 37.2 acres of residential land, of which 16.1 acres are single-family homes, 16.3 acres are multi-family, and 3.8 acres are mobile homes. The area adjacent to the Barton Hospital in particular contains a diverse range of housing types, including single and multi-family dwellings, senior care facilities, and mobile homes. The age and quality of these structures vary widely. The Industrial District contains a number of residential units, which are non-conforming under an industrial designation. The residential units in the plan area, for the most part, tend to be lower-priced rentals rather than owner-occupied housing.

Non-Residential

The Area Plan contains a wide array of commercial uses spread throughout the planning area. Most of these are located in commercial centers along the highways. The area does have several major commercial centers surrounding the Y intersection. These include a center anchored by a Raley's supermarket and Kmart, an outlet store center, and commercial retail property occupied by a TJ Maxx.

The area east of the Y intersection along Lake Tahoe Boulevard contains a car dealership, hardware store, and a tire sales and repair center.

The plan area also contains a mix of light industrial uses concentrated primarily in the northern part of Tahoe Valley. These uses include small-scale manufacturing, a solid waste transfer and recycling facility, auto repair shops, warehousing, wholesale, and other light industrial service uses. Many of these businesses are adjacent to residential uses.

Public and quasi-public uses are discussed in detail under Public/Quasi-Public Facilities and Services.

Vacant Lands

Approximately 18.1 acres (5.4 percent), of the plan area is currently (2014) vacant and held in private ownership. These vacant lots are not concentrated in any one part of the plan area, rather they are scattered throughout the plan area.

Publicly-Owned Open Space

The plan area contains a number of (6 percent, 20.2 acres) of publicly-owned vacant open space parcels. These properties are owned by the United States Forest Service, California Tahoe Conservancy, or City of South Lake Tahoe and are subject to various use and development restrictions based on which agency owns the property and the manner in which the property was acquired.

California Tahoe Conservancy Asset Lands

In the California Tahoe Conservancy's (CTC) land acquisition inventory of 4,890 parcels totaling more than 6,500 acres, there are just over 300 Conservancy-owned properties that are regarded as "asset lands." Asset lands are parcels of land within the Conservancy's ownership which are not essential to carrying out Conservancy goals, which could have significant market value, and which are not otherwise restricted from disposal by law or Board policy. In fact, some asset lands, at the time of acquisition, were identified for possible future transfer or sale. The CTC defines a certain class of lands as "Area Plan or Town Center Asset Lands." These asset lands are considered to be developable lands, or portions thereof, in highly urbanized areas that could support sustainable, compact development or other uses consistent with the goals of the Regional Plan and local area plans (CTC, 2014). There are a total of 17 asset land parcels basin-wide with 6 of those located within the boundaries of the Tahoe Valley Area Plan. The Tahoe Valley Area Plan asset lands account for approximately 16.4 acres and all the parcels are vacant except for one located on Tata Lane. The parcel is currently developed as an offsite parking lot.

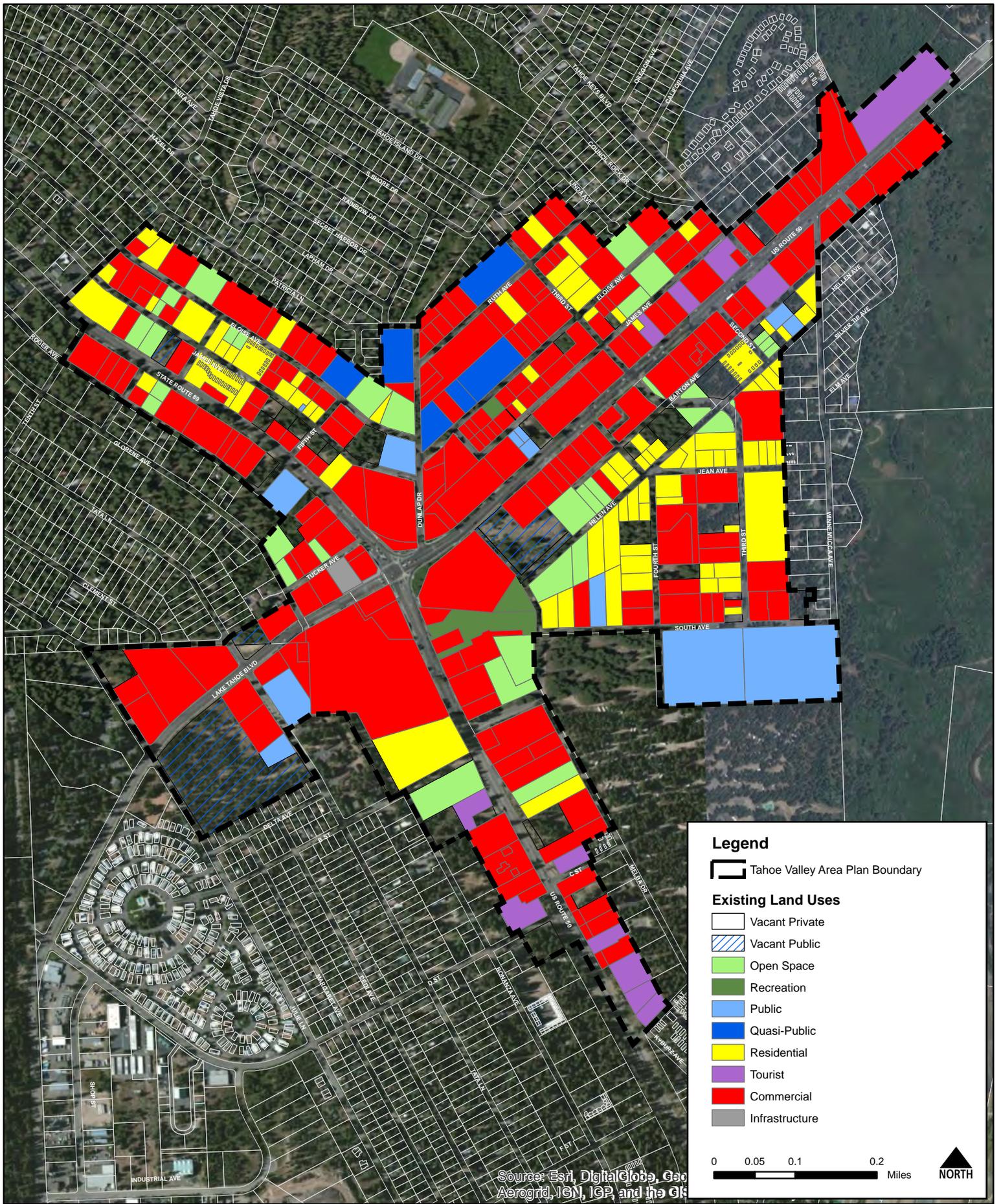
Commercial Floor Area

TRPA regulates the rate and timing of growth within the region, including commercial floor area (CFA). TRPA prohibits the construction of a project or commencement of a use that creates additional CFA without first receiving an allocation from the City or from TRPA. However, TRPA does allow the transfer and reconfiguration of legally existing commercial floor area. Within the Tahoe Valley plan area, there is approximately 1,161,000 square feet of existing developed CFA. An additional 15,000 square feet of CFA reserved for the Area Plan will be made available for use upon adoption of the Tahoe Valley Area Plan.

Lake Tahoe Airport Comprehensive Land Use Plan

As part of the land use planning process mandated by the Division of Aeronautics and the California Airport Land Use Planning Handbook, several Airport Land Use Commissions (ALUCs) have been formed throughout the state to prepare and administer Comprehensive Land Use Plans (CLUPs) for each public-use airport in the state. El Dorado County has an operating ALUC that has prepared CLUPs for the airports in the county. However, the City of South Lake Tahoe has been named as the administrator of the Lake Tahoe Airport. As such, the City operates as its own ALUC and the City has developed the Lake Tahoe Airport CLUP.

The southeastern portion of the TV plan area lies within the Overflight Safety Zone (Safety Zone 3) and under the Horizontal Imaginary Surface of the Lake Tahoe Airport Comprehensive Land Use Plan (CLUP). Safety Zone 3 is the least restrictive Safety Zone around the airport and thus only privately owned assembly and entertainment, collection stations, and sport assemblies are prohibited within that area. The Horizontal Surface, located over the same portion of the plan area as Safety Zone 3, is located at 6,414 feet elevation, more than 100 feet above the highest point in the area and 95 feet higher than the tallest building in the Area Plan.



Legend

Tahoe Valley Area Plan Boundary

Existing Land Uses

- Vacant Private
- Vacant Public
- Open Space
- Recreation
- Public
- Quasi-Public
- Residential
- Tourist
- Commercial
- Infrastructure

0 0.05 0.1 0.2 Miles



Source: Esri, DigitalGlobe, GeoEye, AeroGRID, IGN, IGP, and the GIS User Community



City of South Lake Tahoe, CA
Tahoe Valley Area Plan

Figure 4
Existing Land Use
June 4, 2014

3.3 Transportation and Circulation

Roadways

The Tahoe Valley plan area is served by a variety of arterial, collector, and local streets. The existing road network is shown in Figure 3.

The highway and street network within the plan area is largely dominated by US Highway 50 (Lake Tahoe Blvd.) and SR 89 (Emerald Bay Rd.). US Highway 50 is the major east-west roadway facility connecting Sacramento, California and Carson City, Nevada, and is the main facility serving the south shore area of Lake Tahoe. East of the Y intersection, US Highway 50 is also Lake Tahoe Boulevard. Along most of its 1.5 mile length within the plan area, Lake Tahoe Blvd is a five-lane arterial with a continuous two-way left-turn median lane that transitions to dedicated left-turn pockets at major intersections. In addition to Lake Tahoe Blvd, there is an network of arterial, collector, and local streets that provide the remaining framework for the overall roadway system. These secondary streets serve as the connection points to adjacent neighborhoods as well as serving as access to the Area Plan's Industrial District and Hospital District.

The combination of a relatively limited network of roads along with the high traffic volumes created during peak visitor periods emphasizes the importance of having a well-functioning transportation and circulation system. During peak hours, congestion affects the area.

Level of Service

TRPA's Regional Plan Goals and Policies and Regional Transportation Plan, as well as the City's General Plan, set standards for vehicle "level of service" (LOS). LOS is a qualitative measure that describes the operating performance of transportation facilities. For roadways, LOS is an indicator of traffic flow from the perspective of motorists based on factors such as speed, travel time, delay, freedom to maneuver, volume, and capacity. It is defined on a scale from LOS A, the least congested operating conditions, to LOS F, the most congested operating conditions.

The TRPA Regional Plan and City General Plan require that peak period traffic flow not exceed LOS D for major roadways (arterials and collector routes), and signalized intersections shall be at LOS D. LOS E may be acceptable during peak periods, not to exceed 4 hours per day.

The US 50/SR 89 intersection is operating at LOS C while US 50 roadway segments are operating at LOS E during the summer Friday evening peak hour (TRPA, 2012).

While a number of roadways currently operate at LOS D or E, the key intersections connecting those facilities generally operate at LOS C or better. This is because many of those intersections have been designed with additional travel lanes, channelized "free" right-turn movements, and other capacity enhancements to operations for this segment (TRPA, 2012).

Curbs and Gutters

The City has historically required curb and gutter improvements only for projects requiring the issuance of a Special Use Permit. As a result, much of the Tahoe Valley plan area contains a discontinuous system of curbs, gutters, and roadside stormwater infiltration. Along US 50, Caltrans has recently constructed a water quality improvement project that resulted in the installation of curbs and gutters from the Y intersection to E Street. Installation of curbs and gutters are also scheduled to be installed on SR 89 from the Y intersection heading north in 2014 and on US 50 from the Y intersection to Trout Creek in 2016.

Y Intersection

The Y intersection is the common reference for the intersection of US 50, SR 89, and Lake Tahoe Boulevard. This intersection has historically functioned poorly, during peak tourist periods when traffic volumes are high and when weather-related reductions in roadway capacity or vehicle speeds. During peak periods, southbound traffic on US 50 south of the Y forms queues that back through the Y and east on US 50. While the frequency and length of queue conditions are a function of factors that vary year-to-year and records of queue periods are not kept, a reasonable estimate would be that queues form during a period of one to three hours per day on roughly 20 days per year, or a total of roughly 50 hours per year (CSLT, 2005).

The intersection was improved in 2009 with restriped lanes, improved signalization and an improved right-turn “bypass lane” for the northbound-to-eastbound and the westbound-to-northbound movements, separated from through traffic by a raised island.

Parking Facilities

Most private commercial and industrial businesses provide off-street parking for employees and patrons. However, many of these properties do not meet current parking requirements of Chapter 5 of the City Code. This forces many people to park on roadways and unpaved areas which creates a problem for snow removal and street sweeping equipment. There are presently no off-street publicly-owned parking facilities within the plan area.

Pedestrian and Bicycle Facilities

The City of South Lake Tahoe’s bicycle network is made up of three classes of bicycle facilities (classified by the California Department of Transportation), as follows:

- **Class I Bike Path.** These paths are intended for the exclusive use of bicycles or shared with pedestrians and are physically separated by distance or a barrier from the roadway. Class I paths provide the safest opportunities for and most efficient bicycle travel. Existing Class I facilities in the Area Plan are limited, consisting of relatively short segments south and east of the Factory Outlet Stores at the “Y” intersection and a facility crossing the Upper Truckee River just to the north of US 50 in the eastern end of the Area Plan. A nearby important Class I facility is the facility following SR 89 to Camp Richardson and Spring Creek, which starts at the northern end of Eloise Avenue at 15th Street.
- **Class II Bike Lane.** Class II facilities share the right-of-way with the roadway defined by the creation of a separate lane with pavement markings. Within the Area Plan, these facilities are provided along Tahoe Keys Boulevard, Helen Avenue, Melba Drive, B Street, C Street, 10th

Street, Lake Tahoe Boulevard west of the “Y” intersection, and US 50 from the “Y” intersection just beyond E street.

- **Class III Bike Route.** As with Class II facilities, bike routes share the right-of-way with the roadway; however, they are not separated by marking or barriers, but rather are designated by signage along the roadway. Class III facilities are typically provided along low-volume streets to minimize the potential for conflicts between bicyclists and motorists. In the Area Plan, Class III routes are designated along Eloise Avenue and SR 89 north of the “Y” intersection. This route connects to the Class I path that roughly parallels SR 89 to Camp Richardson and beyond.

The Tahoe Valley plan area contains various Class I, II, and III Bicycle Routes (Figure 5). Where bike facilities are not provided it is common to see pedestrians and bicyclists using the street right-of-ways to navigate the community.

Transit Services and Facilities

South Lake Tahoe, including the Tahoe Valley plan area, is served by BlueGo fixed route bus service and Amtrak buses. A senior van service assists many area seniors in getting to appointments and running errands. The routes of these various transit services are shown in Figure 5.

All fixed route bus stops within the plan area have benches, while some of the stops also provide shelter for bus users. The City has been coordinating with the Tahoe Transportation District on installing additional bus shelters along US 50 as funding becomes available.

The Y Transit Center is located at the southwest corner of the Y intersection. This center is one of two City-owned transit centers, and provides a transfer point for transit users on the BlueGo fixed-route bus services. The transit center is also served by Amtrak buses connecting to the Capitol Corridor train route in Sacramento.

South Lake Tahoe Airport

The Tahoe Valley plan area is located approximately one mile northwest of the Lake Tahoe Airport. The airport, which is a general aviation airport owned and operated by the City of South Lake Tahoe, contains a single north-south 8,541 foot long runway. The airport facility includes a commercial passenger terminal, 169 plane tie-down spaces, and a 57-unit hanger and storage complex.

There were 28 aircraft based at the airport as of May 2014. There were 23,540 annual operations counted by the FAA tower in 2010, which is a significant decline from the 63,800 annual operations

3.4 Public/Quasi-Public Facilities and Services

This section describes the public and quasi-public facilities and services located in the Tahoe Valley plan area. In addition to these, the area hosts several cellular towers, offices for the local power and gas suppliers, day care centers, places of worship, and social service organizations.

Water and Wastewater Services

Water service within the plan area is provided by the South Tahoe Public Utilities District (STPUD) and Lukins Brothers Water Company (Lukins). STPUD provides water service to the majority of the plan area.

Lukins provides water services to those parcels located north of Emerald Bay Road and west of Dunlap Drive.

STPUD is a California Special District established in 1950. The 27,000-acre service area covers the south shore of Lake Tahoe from Emerald Bay on the west, Christmas Valley on the south, the California-Nevada state line to the east, and Lake Tahoe to the north. The water supplied by the District comes entirely from 20 active municipal wells that pump groundwater from the Lake Tahoe Basin. The District maintains one pump station and one active groundwater well within the plan area.

The Lukins Brothers Water Company is a private water purveyor established in 1942 which is governed by the California Public Utility Commission. They have 950 service connections, approximately 200 of which are within the Tahoe Valley plan area and include businesses (75 percent), single-family (5 percent) and multi-family (20 percent) dwellings.

STPUD collects and treats wastewater from businesses and residents within the District's service area, which encompasses approximately 42 square miles and includes the City of South Lake Tahoe and unincorporated area of El Dorado County within the Tahoe Basin. The District's wastewater collection/treatment/recycling processes include a maximum 7.7 million gallons per day (mgd) advanced secondary operation serving 17,000 customers. The District's collection system and wastewater treatment plant produces an average dry day rate of 4.3 mgd, 100 percent of which is recycled. Flows are conveyed to the District's Wastewater Treatment Plant (WWTP). The District's WWTP, last expanded in 1989, has a capacity 7.7 mgd. Treated effluent is exported 26 miles to Harvey Place Reservoir in Alpine County (STPUD, 2007 & 2014).

Electricity Services

Liberty Utilities provides electrical services to the plan area. Service is primarily transmitted over existing overhead lines along collector and local streets and underground along arterials (i.e., US 50, SR 89 and Lake Tahoe Boulevard). Liberty Utilities' office is also located within the Area Plan on Eloise Avenue.

Gas Services

Natural gas service in the Tahoe Valley area is provided by Southwest Gas, which has been providing gas to the South Lake Tahoe area since 1963. Southwest Gas is regulated by the California Public Utilities Commission for the service they provide in California.

Telephone and Cable Services

AT&T, Verizon, and T-Mobile provide telephone and cellular service to the plan area. Charter Cable provides cable television, phone and internet services. A number of cellular towers are located within the plan area.

City of South Lake Tahoe Facilities

- **City Services Center.** Located at 1052 Tata Lane, this facility is owned by the City and houses the Development Services Department and Engineering Division of the Public Works Department.

- **Fire Station #3.** Located at 2101 Lake Tahoe Boulevard, this facility is owned by the City and is one of three fire stations that serve South Lake Tahoe. This station serves the Tahoe Valley area with response times between three and five minutes.
- **Transit Center.** The transit center, owned by the City of South Lake Tahoe and located at the “Y” intersection, provides shelter and a transfer location for the BlueGo bus system, and a pickup/drop-off point for Amtrak buses.

El Dorado County Facilities

- **El Dorado County Department of Transportation** Located at 924 Emerald Bay, this privately-owned facility is rented by the County.

State Facilities

- **California Tahoe Conservancy (CTC).** Located at 1061 Third Street, the Conservancy is an independent State agency within the State Resource Agency. It was established in its present form by State law in 1984 to develop and implement programs through acquisitions and site improvements to improve water quality in Lake Tahoe, preserve the scenic beauty and recreational opportunities of the region, provide public access, preserve wildlife habitats, and manage and restore natural lands. Conservancy staff is housed in a privately-owned, rented building.

Federal Facilities

- **Lake Valley Post Office.** Located at 950 Emerald Bay Road, this post office provides post office boxes for the 96158 zip code and full postal services to customers out of a privately-owned rented building.
- **Tahoe Resource Conservation District.** Located at 870 Emerald Bay Road, the Tahoe Resource Conservation District promotes the conservation, stewardship and education of the Lake Tahoe Region’s natural resources.

Barton HealthCare System Facilities

Barton Memorial Hospital has served the South Lake Tahoe community since 1963 as a regional medical center. Part of the Barton HealthCare System, Barton Hospital, is South Lake Tahoe’s largest medical facility and largest employer (900 employees). The hospital and its associated medical clinics and emergency medical centers perform approximately 3,180 surgeries (2,800 within the City limits and 380 outside the City limits) and 20,000 emergency visits per year (Lyn Barnett, per. comm. 4/17/2014).

South Tahoe Refuse

South Tahoe Refuse has franchise agreements with the City of South Lake Tahoe, El Dorado County, and Douglas County for the collection and transportation of solid waste to processing and/or disposal facilities. This includes waste from all residential, commercial, and industrial properties, including recyclable material. These facilities support the processing of this waste before its ultimate disposal outside of the Tahoe Basin. South Tahoe Refuse operates the following facilities:

- **Administrative Office/Transfer Station/Materials Recovery Facility/Tahoe Basin Container Service.** All located at 2140 Ruth Avenue.
- **Recycling Center.** Located at 2192 Ruth Avenue.

3.5 Natural and Cultural Resources

Natural resources within the Tahoe Valley plan area include the Upper Truckee River, natural and landscaped vegetation, stream environment zones, and views of distance mountain ridgelines. The plan area includes three drainage basins including the Tahoe Valley system that runs south of the “Y” intersection, which is directly, connected to the Truckee River. The Other two drainage basins drain into the Tahoe Keys Lagoons before being released into Pope Marsh. Many properties contain excess coverage over the Bailey land capability limitations.

Native vegetation consists primarily of Jeffrey pine and white fir trees with little under story vegetation on high capability lands and willows and grasses on low-capability lands. Many of the developed parcels are also landscaped with native vegetation. Stream Environment Zones within the plan area generally have been disturbed.

Land Capability and Stream Environment Zone

The land capability system established by Bailey is the basis of the land coverage standards and limitations set forth in Chapter 30 of the TRPA Code of Ordinances. Chapter 30 uses land capability districts (LCDs) ranging from class 1 to 7 that assign a percentage of allowable land coverage in a designated LCD area as identified below:

- Land capability classes 1a-c and 2 are not suited for urbanization or intensive forestry use, but are suited for open space, conservation areas, and low-intensity recreation.
- Land capability classes 3 and 4 are not suited for urbanization but are suited for forestry and low-density housing use.
- Land capability classes 5, 6, and 7 include land suitable for urbanization and other uses.

Tahoe Valley is predominately flat, and according to the TRPA’s mapped land capability map, mostly comprised of high capability lands (LC 5 – 7) suitable for re-development and infill (207.5 acres). The plan area includes three mapped primary areas of environmentally sensitive or SEZ lands accounting for approximately 122.4 acres. SEZs can be found in vicinity of James Avenue and Seventh Avenue, and just north and south of the “Y” intersection (see Figure 6). According to TRPA’s land capability map just over a third (36.4%) of the plan area is SEZs with the remaining areas being high capability lands (63.4%). Table 2 below provides an accounting of mapped land capability district in the plan area.

As noted below, mapped SEZs account for approximately 36.4% of the plan area. However, upon further investigation and field analysis conducted by soil scientists, preliminary information indicates that portions of the SEZs that run through the plan area may have been mapped erroneously and accounts for a smaller area than that depicted in TRPA’s Land Capability Overlay Map. This issue is discussed in further detail in the Section 10, Natural and Cultural Resources.

Table 2 Mapped Land Capability District		
Land Capability District	Total Acreage	Percent
1B	122.4	36.4%
5	6.6	2.0%
7	207.5	61.7%
Totals	336.5	100.0%

Source: TRPA Land Capability Overlay Maps (1"=400'), 1987, and TRPA GIS, 2014.

Land Coverage

An analysis of TRPA's LiDAR impervious coverage data estimates existing land coverage in the plan area at approximately 52.2% (175.7 acres). Hard coverage accounts for 49.5% (167.3 acres) and soft coverage accounts for 2.5% (8.4 acres). The majority of existing coverage in the plan area is attributable to parking lots (55.4%) with the remainder equally divided between building structures (22.4%) and paved roadways (21.0%). Just over two-thirds of existing coverage (117.0 acres, 66.6%) is located on high capability lands with the remaining third located in mapped SEZs (58.7 acres, 33.3%). An accounting of the existing land coverage conditions in the Area Plan is presented in Table 3 below and Figure 7.

Table 3 Existing Land Coverage (acres)							
Coverage Type	Roads	Trails	Buildings	Other	Total Impervious	Total Area Plan Acreage	Percent Impervious
Hard	37.0	1.8	39.3	89.2	167.3	336.5	49.7%
Soft	0.0	0.4	0.0	8.0	8.4		2.5%
Totals	37.0	2.2	39.3	97.2	175.7		52.2%
Percent	21.0%	1.2%	22.4%	55.4%			
Existing Land Coverage by Land Capability District (acres)							
Land Capability	Total Impervious				Percent Impervious		
1B	58.7				33.4%		
5	3.6				2.0%		
7	113.5				64.6%		
Totals	175.7				100.0%		

Source: TRPA GIS Impervious LiDAR Dataset, 2010.

Water Quality

Lake Tahoe is Federally-designated as an Outstanding National Resource Water pursuant to the provisions found in the Clean Water Act (CWA). The Act states that "[w]here high quality waters constitute an outstanding National resource, such as waters of National Parks, State parks and wildlife refuges, and waters of exceptional recreational or ecological significance, that water quality shall be maintained and protected." Its designation provides the maximum amount of protection to the water quality of Lake Tahoe under the CWA, ensuring that no permanent degradation of water quality will occur.

Stormwater Drainage

The City and California Department of Transportation (Caltrans) are responsible for reducing erosion and storm water runoff from roads under their jurisdiction. This includes installing curbs, gutters, rock-lined channels, bio-swales, infiltration basins and other improvements that captures runoff and reduces fine sediment loads to Lake Tahoe.

The Tahoe Valley plan area contains a series of natural and human-made systems constructed by the City to convey and treat drainage. The area to the north of the “Y” intersection is collected and treated by two drainage systems (i.e., Gardner Mountain Catchment and Eloise Catchment) that are comprised of conveyance pipes, rock lined ditches, and basins. The outfall for the two catchments is the Tahoe Keys lagoon located between Whitney Drive and Lassen Drive. The lagoon drains into Pope Marsh before entering Lake Tahoe. The area to the south of the “Y” intersection (Upper Truckee River Catchment) is treated by the Tahoe Valley Erosion Control Project. The system collects and treats runoff from much of the densely developed commercial areas near US 50, both north and south of the “Y” intersection, and from the Bonanza and Barton Hospital neighborhoods. Storm water is conveyed with a series of pipes and rock lined ditches, where it is treated in the vicinity of James Avenue and Tahoe Keys Boulevard, before flowing into a ditch north of Sky Meadows and out to the Truckee River. This drainage basin is a high priority for storm water runoff treatment and the City is currently in the planning stages to update the system and design water quality improvements to reduce pollutant loads discharged to the Upper Truckee River.

Storm water runoff along US 50 and SR 89 is the responsibility of Caltrans. Caltrans is in the process of constructing a water quality improvement project that runs from the “Y” intersection to the Lake Tahoe Airport. The first mile of project running from the “Y” intersection to E Street was constructed in the summer of 2013. The improvement project will collect and treat runoff along US 50 and also includes the installation of curb, gutter and sidewalks, and the widening of the roadway to accommodate a Class II bike lane. Construction of water quality improvements along SR 89 is scheduled for 2014 and along US 50 from the “Y” intersection to Trout Creek Bridge in 2016.

Figure 8 depicts the existing water quality improvement projects within the plan area.

Best Management Practices

Every property within the Tahoe Valley Area is subject to the requirements of Chapter 60 of the TRPA Code of Ordinances for drainage treatment, paved parking and drives, slope stabilization, revegetation, and providing snow storage areas. The Code also requires property owners to infiltrate the volume of a 20-year/1 hour storm on their property or meet alternative standards in instances where special circumstances limit infiltration. All projects permitted by the City under the Memorandum of Understanding (MOU) with TRPA are reviewed for compliance with Chapter 60. TRPA is responsible for BMP enforcing and permitting retrofits on existing development

According to TRPA’s BMP database the plan area has a 32% BMP compliance rate for BMP certification. Based on TRPA’s BMP database a total of 127 out of 400 parcels have been issued their Certificate of Compliance from TRPA. The compliance rate for commercial uses located within the Area Plan is 47%. It should also be noted that the BMP compliance rate is based on issuance of a BMP certificate and not on actual BMP installation which is a permit requirement for all project approvals. Therefore, the actual rate of BMP installation is higher than the 32% compliance rate.

Total Maximum Daily Load (TMDL)

The TMDL program is a requirement of the Federal Clean Water Act. The program is designed to protect Lake Tahoe, an Outstanding Natural Resource Water, from certain pollutants of concern, including fine sediments, phosphorus and nitrogen.

Through the Lake Tahoe TMDL, the Lahontan Regional Water Quality Control Board (Lahontan) established five-year load reduction targets to assess each jurisdiction’s progress towards meeting overall load reduction goals. Load reduction targets for Fine Sediment Particles (FSP), Total Phosphorus (TP), and Total Nitrogen (TN) have been established based on attainment of California’s Lake Tahoe transparency standard (~ 97 feet) over an estimated 65-year implementation period. To meet the requirements of the TMDL Program, each jurisdiction holding a National Pollutant Discharge Elimination System (NPDES) permit is required to reduce the baseline pollutant load within their jurisdiction by the amounts shown in Table 4.

Table 4 LOAD REDUCTION TARGETS FOR THE TAHOE TMDL				
Pollutant	5-Year Milestone 2016	10-Year Milestone 2012	15-Year Milestone 2026	Transparency Standards (~65-Years)
Fine Sediment Particles	10%	21%	34%	71%
Total Phosphorus	7%	14%	21%	50%
Total Nitrogen	8%	14%	19%	46%

Source: Final Lake Tahoe Total Maximum Daily Load, August 2011

In 2011, the City submitted to Lahontan the City’s *Lake Tahoe TMDL Baseline Pollutant Load Estimate Report* which was subsequently incorporated into the City’s NPDES permit. The City’s load reduction requirement through 2016 under the NPDES permit is shown in Table 5.

Table 5 CSLT 2016 LOAD REDUCTION REQUIREMENTS							
Parameter	Baseline Load (kg/year)	Required Reduction Percent	Required Load Reduction (kg/year)	Allowable Load (kg/year)			
Fine Sediment Particle (mass)	176,450	10%	14,650	158,800			
Fine Sediment Particles (# of particles)	1.94E+19	10%	1.94E+18	1.75E+19			
Total Phosphorus	789	7%	55	734			
Total Nitrogen	3,361	8%	269	3,092			

Source: City of South Lake Tahoe Pollutant Load Reduction Plan, January 2013.

The City authorized development of a *Pollutant Load Reduction Strategy Report* to assess potential approaches for reducing pollutant loading to Lake Tahoe from urban stormwater runoff. The goal of the Strategy Report was to identify feasible and cost effective actions to meet anticipated targets. Subsequently, in February 2013, the City Council adopted the *City of South Lake Tahoe Pollutant Load Reduction Plan* which documents the City’s proposed approach and timeline for meeting the 2016 load reduction targets set forth in the City’s NPDES permit. Lahontan subsequently approved the City’s Load Reduction Plan in June 2013. Proposed strategies outlined in the Load Reduction Plan for meeting the load reduction targets are incorporated in the Area Plan and are discussed in detail in Section 10, Natural and Cultural Resources, and Section 10, Implementation.

Underground Sources of Potential Water Quality Concerns

South Tahoe Public Utility District’s system includes 1,700 sewer laterals (the portion of the sewer system connecting the building to the main). Twenty-two percent of these are over 40 years old, 39 percent are 30 to 40 years old, 24 percent are 20 to 30 years old, and only eight percent are less than 10 years old. While a lateral can last 50 to 100 years, its life expectancy is determined by its material, original installation, root intrusion or external pressure, soil stability and chemical makeup, high water tables, corrosion (sometimes from the hydrogen sulfide gas present in the sewer system), and forces leading to structural failure.

Source Water Considerations

TRPA-designated “source water protection zones” overlay much of Tahoe Valley. These protection zones consist of a fixed 600-foot radius circle around wells that provide drinking water from an aquifer with five or more service connections. Source Water Protection Zones were broadly mapped by TRPA in 1999. Table 6 shows the wells on TRPA’s Source Water Assessment Map (12-2-99) that are located in the plan area and Figure 9 shows their locations.

Table 6 TRPA Source Water Wells		
APN/Well Number	Status	Name/Owner
02316106W11	Active	Washoe Motel
02316133W11	Active	CMO Properties
02318201W11	Active	Bel Pac South
02318131W11	Active	Well #3 (Lukins Bros)
02319107W11	Active	Crystal Range Motel
02308110W11	Inactive	South Y Well (STPUD)
03214106W11	Active	Tahoenian Apartments
03220112W11	Active	Matterhorn Motel
03220104W11	Active	Tahoe Sundowner
02321141W11	Active	Matterhorn Motel
03104403W11	Inactive	Helen Ave. #1 (STPUD)
03104404W11	Inactive	Helen Ave. #2 (STPUD)
02322139W11	Inactive	Little Truckee MHP

Table 6 TRPA Source Water Wells		
APN/Well Number	Status	Name/Owner
02365518W11	Active	Well #4 (Lukins Bros)
02380215W11	Inactive	Clement Well (STPUD)
03230202W11	Inactive	Julie Well (STPUD)
03224212W11	Inactive	Tata Lane #3 (STPUD)
03224211W11	Inactive	Tata Lane #2 (STPUD)
03214106W11	Active	Turney Building
Source: TRPA Source Water Assessment Map, December 1999.		

TRPA Code [Section 60.3.5] defines “possible contaminating activities” as activities equivalent to TRPA primary uses identified by either the California Department of Health Services or the Nevada Bureau of Health Protection Services, regardless of where the project is located, as having the potential to discharge contaminants to surface or groundwaters. Some of the existing and permissible industrial uses in the Industrial District qualify under this definition. New project applications for uses that constitute possible contaminating activities within these zones must make findings based on criteria established in Chapter 60 of the TRPA Code in order to be approved. The following uses are considered possible contaminating uses in Section 60.3.5 of the TRPA Code:

Residential

1. Domestic animal raising

Commercial

1. Service stations
2. Auto repair and service
3. Business support services
4. Laundries and dry cleaning plant
5. Repair services
6. Batch plants
7. Fuel and ice dealers
8. Industrial services
9. Recycling and scrap
10. Storage yards
11. Vehicle storage and parking
12. Vehicle and freight terminals

Public Service

1. Airfields, landing strips, and heliports
2. Collection stations
3. Hospitals
4. Local public health and safety facilities
5. Regional public health and safety facilities
6. Power generating
7. Public utility centers
8. Schools

Linear Public Facilities

1. Transit stations and terminals

Recreation

1. Beach recreation
2. Boat launching facilities
3. Developed campgrounds
4. Golf courses
5. Marinas
6. Recreational vehicle parks
7. Rural sports

Resource Management

1. Timber stand improvement
2. Grazing
3. Range pasture management

Watershed Improvements

1. Runoff control

Shorezone

1. Construction equipment storage
2. Seaplane operations
3. Tour Boat operations
4. Water-oriented outdoor recreation concessions

Businesses with fuel tanks or other potentially contaminating materials used as part of their operations are required to have spill contingency plans that address how to respond to accidental spills. Plans meeting specific content requirements are required for potentially contaminating activities located within a Source Water Protection Zone.

Cultural Resources

Eight archaeological and historical investigations were previously conducted within the Tahoe Valley plan area. These investigations identified historic sites CA-Eld-721-H and CA-Eld-2240-H. Site CA-Eld-721-H is a portion of the Pony Express Trail that generally follows the alignment of U.S. Highway 50. Site CA-Eld-2240-H is a historic road that passes through the plan area. In addition, a heritage resources study was conducted specifically for the Area Plan (Lindström and Rucks, 2006). This study did not include formal archaeological survey or architectural inventory of the Tahoe Valley area, but did identify areas that are important to the Native American community and potentially significant historic buildings and structures (e.g., motels/auto courts) within the Area Plan. In addition, the Barton Ranch is located within the plan area at 1080 Emerald Bay Road and is listed on TRPA's Historic Sites Maps (1984).

The study concluded in more recent times, the "Y" appears to have been a central point for historical interactions between Washoe and non-Washoe and it seems to continue to represent this period of adjustment in Washoe history. Interviews with Washoe participants demonstrated that the "Y" has remained an important travel corridor, meeting place, and access point to post-contact Washoe camps

and work opportunities associated with ranching. Although the study indicates no point specific Washoe “sites” within the plan area, it did recommend that project planning should be sensitive to traditional Native American values and recommends on-going communication with the Washoe Tribe to determine the significance of historic buildings and sites (Lindstrom & Rucks, 2006).

The Mosher Family who owns the Barton Ranch commissioned a report, entitled “Historic Architectural Resources Studies W.D. Barton Ranch,” in February 2009. The report provides information on the historical context of Barton Ranch and its role in Lake Tahoe ranching and Native American significance. The buildings are associated with the livestock and dairy industries in the Tahoe Basin, an important era in the history of Lake Tahoe. The property is also associated with an important Lake Tahoe family, the Bartons, who were longtime ranches in the area, and who donated the land for the Barton Hospital, which is located within the plan area. The report concluded that the Barton Ranch Complex is eligible for listing on the National and California Registers at the local level of significance and could serve as a historical centerpiece if the adaptive reuse of the ranch can be economically viable and the interest of the property owners are protected (Marvin, 2006)

Recreation

There are no City-owned or -operated recreational facilities within the plan area. Informal recreational activities available within Tahoe Valley include bicycling, running, jogging, walking along city streets and designated routes and paths.

South Tahoe High School (located just east of the Tahoe Valley plan area) has tennis courts, ball fields, and a gymnasium. Tahoe Valley Elementary School (located just north of the Tahoe Valley plan area) has play areas. Forest lands managed by the Forest Service located nearby provide opportunities for active and passive outdoor recreation. The plan area is the gateway to the Forest Service’s South Shore Recreation Area, which contains beach access, bike trails, the Tallac Historic Site, a Visitor’s Center (with its Stream Profile Chamber and newly-reconstructed amphitheater), and Camp Richardson Resort. Located within three miles of the “Y” intersection, all of these areas provide for some of the recreation needs of the nearby residential neighborhoods, as well as for visitors, and are accessible by bike routes and bike paths that either run through or adjacent to the plan area (See Figure 5).

Scenic Resources and Community Design

The Tahoe Valley plan area consists of 482 public and privately owned parcels that include residential, commercial, tourist accommodation, industrial, and public service land uses. Land uses to the west, north, and northeast of the plan area consist of residential subdivisions dating from earlier development of the area during the 1960s through the 1990s. Immediately adjacent to the plan area to the east lies a large open space area (stream environment zone [SEZ]) consisting of the floodplain for the Upper Truckee River, which is near the eastern boundary of the plan area. Commercial uses are the primary land use in the plan area and include clothing stores, factory outlets, automotive supply stores and dealerships, drug and liquor stores, restaurants, and supermarkets. Buildings and other structures in the plan area range in age and physical condition from the 1890s to recent construction. They are provided in strip development along US 50 and SR 89. Identified issues for the plan area’s current form of development include many curb cuts for driveway access, small and fragmented parcels, stand-alone structures, old motels, sprawling parking lots, and lack of coordinated sidewalks, bike paths, landscaping, and other related amenities.

As described above, the existing visual landscape characteristics of the plan area consist of urban development intermixed with forest conditions. Existing urban development transitions into natural and forest conditions at its southern boundary along the US 50/SR 89 corridor as well as at the northern boundary along SR 89. The eastern boundary transitions to the open stream environment zone area associated with the Upper Truckee River. From within the plan area views of mountains looking east towards Heavenly Mountain Ski Resort (Gunbarrel ski run is visible) as well as views of the Tahoe Mountain looking west are visible from US 50 and the “Y” intersection. It should be noted that these views are currently not identified as TRPA designated resources.

TRPA has adopted three separate regional scenic resource thresholds: 1) Travel Route Ratings; 2) Scenic Quality Ratings of individual resources; and 3) Scenic Quality Ratings of individual resources seen from public recreation areas. All are numerical in nature. The first two thresholds apply to scenic resources visible from the Region's state and federal highways and Pioneer Trail and from Lake Tahoe itself. The 2011 Threshold Evaluation concluded that regionally, all three threshold indicators are “at or somewhat better than target.” (TRPA, 2012a).

Roadway Travel Units

The plan area contains a portion of Roadway Travel Route Unit #1, Tahoe Valley, a portion of Roadway Unit #35, Al Tahoe, and Roadway Unit #36A, Airport Area. All three roadway units are currently out of attainment; however, Unit #1 and Unit #36A are near attainment. The 2011 Threshold Evaluation noted that recent installation of curbs, gutters, and sidewalks have improved the road structure component score in Unit #35. Recent constructed improvements along US 50 and SR 89 and those planned in the near future are expected to improve the overall threshold rating scores in the near future. Figure 10 depicts the locations of the Roadway Travel Units and Table 7 below for the current threshold rating scores.

TABLE 7 THRESHOLD TRAVEL ROUTE AND SCENIC QUALITY RATINGS							
Unit	1982	1991	1996	2001	2006	2011	Needed for Attainment
1	11.0	11.0	12.0	12.0	12.0	12.0	+3.5
35	7.0	7.5	7.5	7.5	8.0	8.5	+7.0
36A ¹	0.0	0.0	0.0	10.5	13.0	13.0	+2.5

Source: TRPA 2011 Threshold Evaluation.

1. In 2001, Roadway Unit 36 was segmented into three sub roadway units: 36A, 36B and 36C.

As identified above, the three roadway units along SR 89 and US 50 are identified for scenic quality improvement by the TRPA in the Scenic Quality Improvement Program (SQIP). While the TRPA 2011 Threshold Evaluation recognizes these roadway units as have seen some improvements associated with recent development, it calls for continual implementation of the SQIP recommendations at the project level to ensure continual scenic improvements.

Scenic Quality Ratings

There are only two designated resources within the boundaries of the Area Plan: Scenic Resource #35-1 and #36-7. Both resources are designated views of natural landscapes as seen from US 50. Both resources have low scenic quality rating of 1 because of the dominance of the built environment. Both resources are currently in attainment. While there are no prominent designated scenic resources in the plan area, there is the following scenic resource located adjacent to the planning area that has a moderate rating:

- Scenic Resource 35.2: Views of the Truckee River and associated stream environment zone on both sides of US 50.

Scenic Resource Visible from Public Recreation Area and Bike Trails

The 1993 Lake Tahoe Scenic Resource Evaluation established numerical standards for the scenic quality of lands visible from designated public recreation area and bike trails. Within the plan area there are no designated recreation sites but there are two identified bike trails: the Tahoe Valley route and Tahoe Valley to City Limits route (see Figure 10).

The Tahoe Valley route serves uses located south and east of US 50. The route runs along Helen Street from Winnemucca Street towards the “Y” intersection, then heads south along Melba Street and ends at the US 50 junction. The route runs primarily through residential neighborhoods and connects users to the commercial node at the “Y” intersection.

The Scenic Resources Evaluation identified two views of the natural landscape visible from the bike route. Resource:

- Scenic Resource 9-1: Landscape view of scattered forest vegetation interspersed with residential and commercial uses
- Scenic Resource 9-2: Infrequent views of mountain backdrop.

This segment of bicycle route is not of high scenic quality with both views rated as fair. Its primary value is as an alternative transportation route along US 50 for visitors and residents. According to the 2011 Threshold Evaluation, these scenic resources are still in attainment

Open Space

Open space in the planning area includes both undeveloped private lands, environmentally-sensitive public lands, and lands used for stormwater treatment. The plan area contains 36 parcels (20.2 acres) of land that are currently in open space (Table 1). This accounts for 6 percent of the total land in the plan area.

Public Health and Safety

Flooding

According to the National Flood Insurance Program Map for the City of South Lake Tahoe, the majority of the Tahoe Valley plan area lies outside the 100-year floodplain. Some areas located adjacent to the Upper Truckee River, east of the plan area, are included within Zone X and Zone AE. Zone AE is identified

on FEMA maps as subject to inundation by the 1-percent-annual-chance flood event. Zone X are areas of moderate flood hazard from the principal source of flood in the area. Portions of the plan area impacted by Zone X and Zone AE include a small portion of Helen Avenue, Truckee Drive south of Lake Tahoe Boulevard, and east of James Avenue in the vicinity of Motel 6. The remainder of the plan area is located outside the 100-year floodplain.

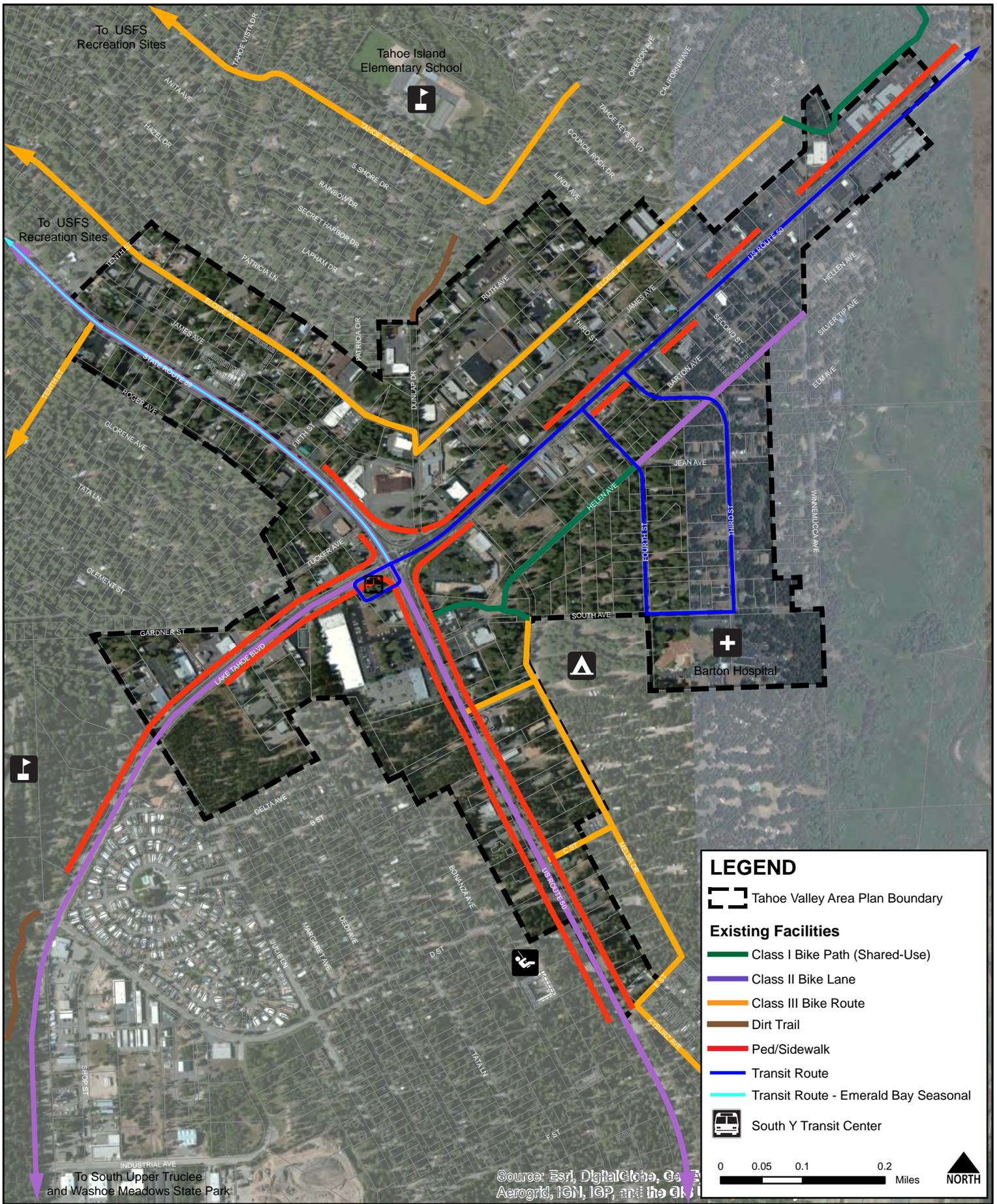
Noise

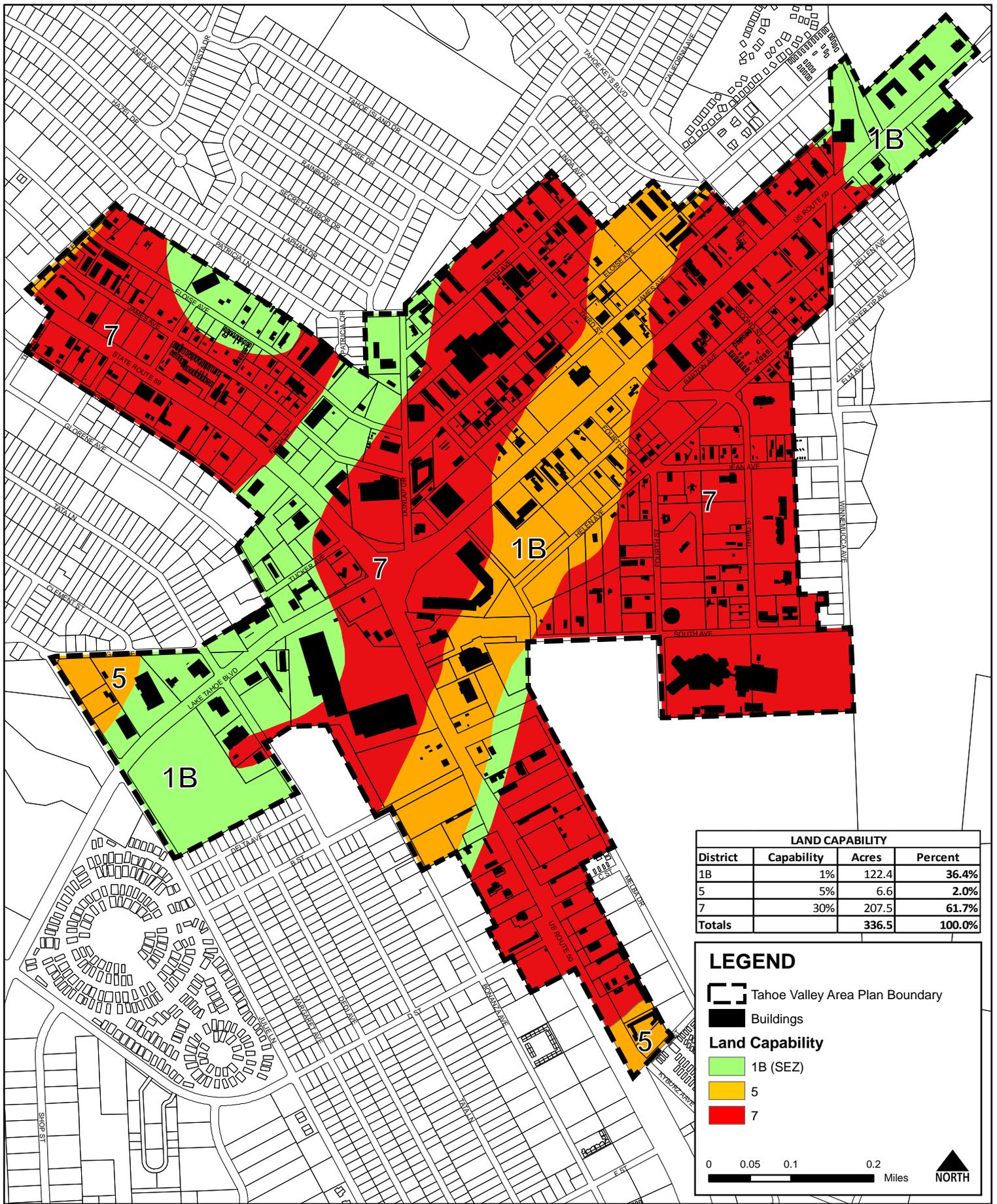
TRPA has adopted noise standards for the Tahoe Basin. The noise thresholds are Community Noise Equivalent Level (CNEL) values for the various land use categories and single event (Lmax) standards for specific noise sources. CNEL is the metric used by TRPA for determining land use compatibility. The maximum CNEL standard for the Tahoe Valley Area is 65, except for the Healthcare District, where 55 CNEL is the standard. The maximum community noise equivalent level for the SR 89 and US 50 corridors is 65 CNEL. The CNEL threshold of 65 dBA that applies to industrial areas was adopted as a result of the 1996 threshold evaluation recommendations. No one activity, nor combination of activities, is allowed to exceed the applicable CNEL level. CNELs are calculated pursuant to Chapter 23 of the TRPA Code of Ordinances.

While the airport does not fall within the boundaries of the Area Plan, its associated overflight zones and noise contours do. According to the Comprehensive Land Use Plan (CLUP) for Lake Tahoe Airport (adopted July 1990, amended December 2004), the 65 dB CNEL contour extends into a portion of the plan area.

Fire

Fire Station #3 is located within the plan area at 2101 Lake Tahoe Boulevard. The facility is owned by the City and is one of four that serve South Lake Tahoe. This station services the Tahoe Valley area with response time between three and five minutes. The Fire Department also provides local ambulance service, and has mutual aid agreements with other fire and emergency response agencies in the Tahoe Region.





LAND CAPABILITY			
District	Capability	Acres	Percent
1B	1%	122.4	36.4%
5	5%	6.6	2.0%
7	30%	207.5	61.7%
Totals		336.5	100.0%

LEGEND

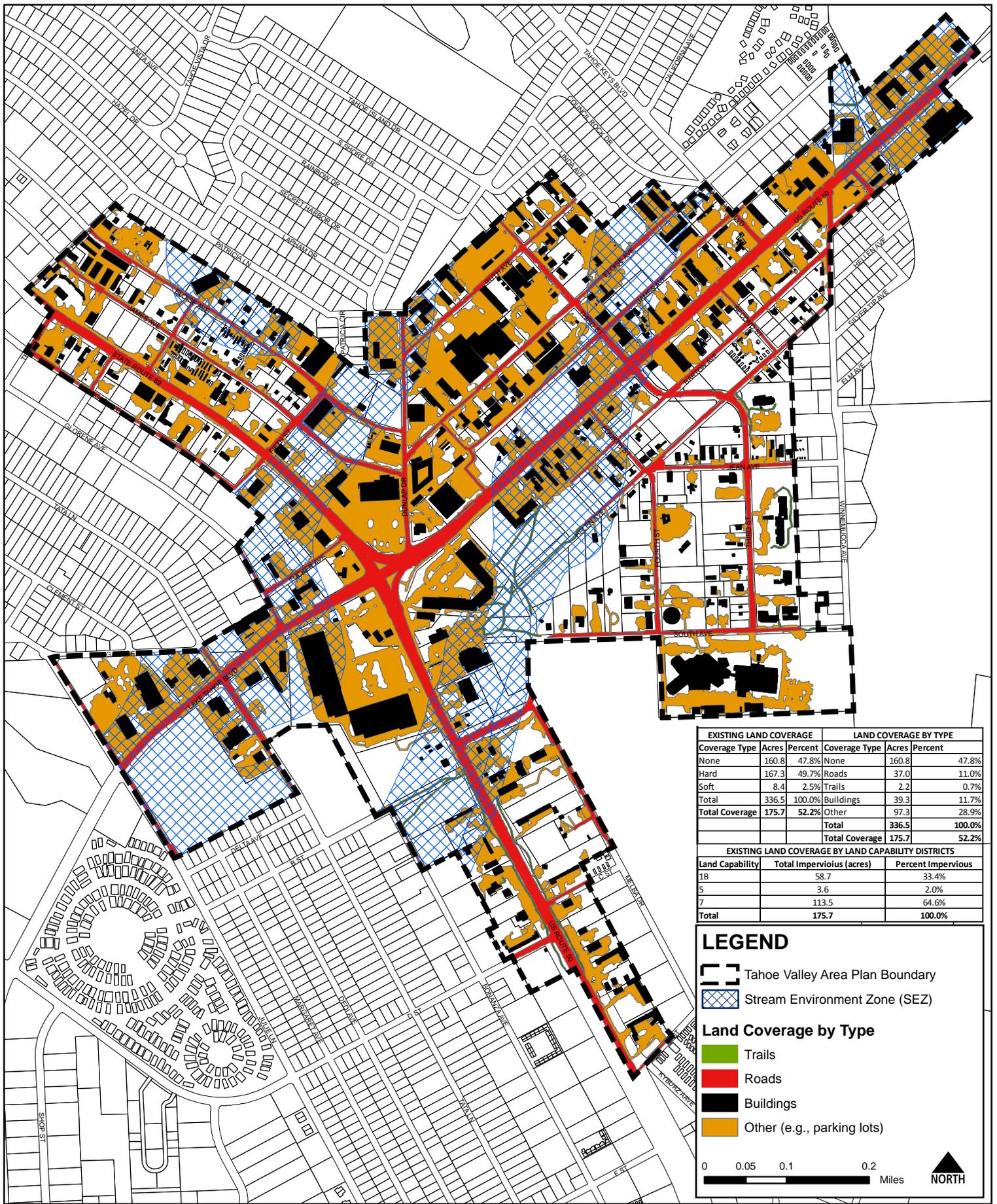
- Tahoe Valley Area Plan Boundary
- Buildings
- Land Capability**
- 1B (SEZ)
- 5
- 7

0 0.05 0.1 0.2 Miles NORTH



City of South Lake Tahoe, CA
Tahoe Valley Area Plan

Figure 6
Mapped 1987 Land Capability Districts
June 4, 2014



EXISTING LAND COVERAGE			LAND COVERAGE BY TYPE		
Coverage Type	Acres	Percent	Coverage Type	Acres	Percent
None	160.8	47.8%	None	160.8	47.8%
Hard	167.3	49.7%	Roads	37.0	11.0%
Soft	8.4	2.5%	Trails	2.2	0.7%
Total	336.5	100.0%	Buildings	39.3	11.7%
Total Coverage	175.7	52.2%	Other	97.3	28.9%
			Total	336.5	100.0%
			Total Coverage	175.7	52.2%

EXISTING LAND COVERAGE BY LAND CAPABILITY DISTRICTS		
Land Capability	Total Impervious (acres)	Percent Impervious
1B	58.7	33.4%
5	3.6	2.0%
7	113.5	64.6%
Total	175.7	100.0%

LEGEND

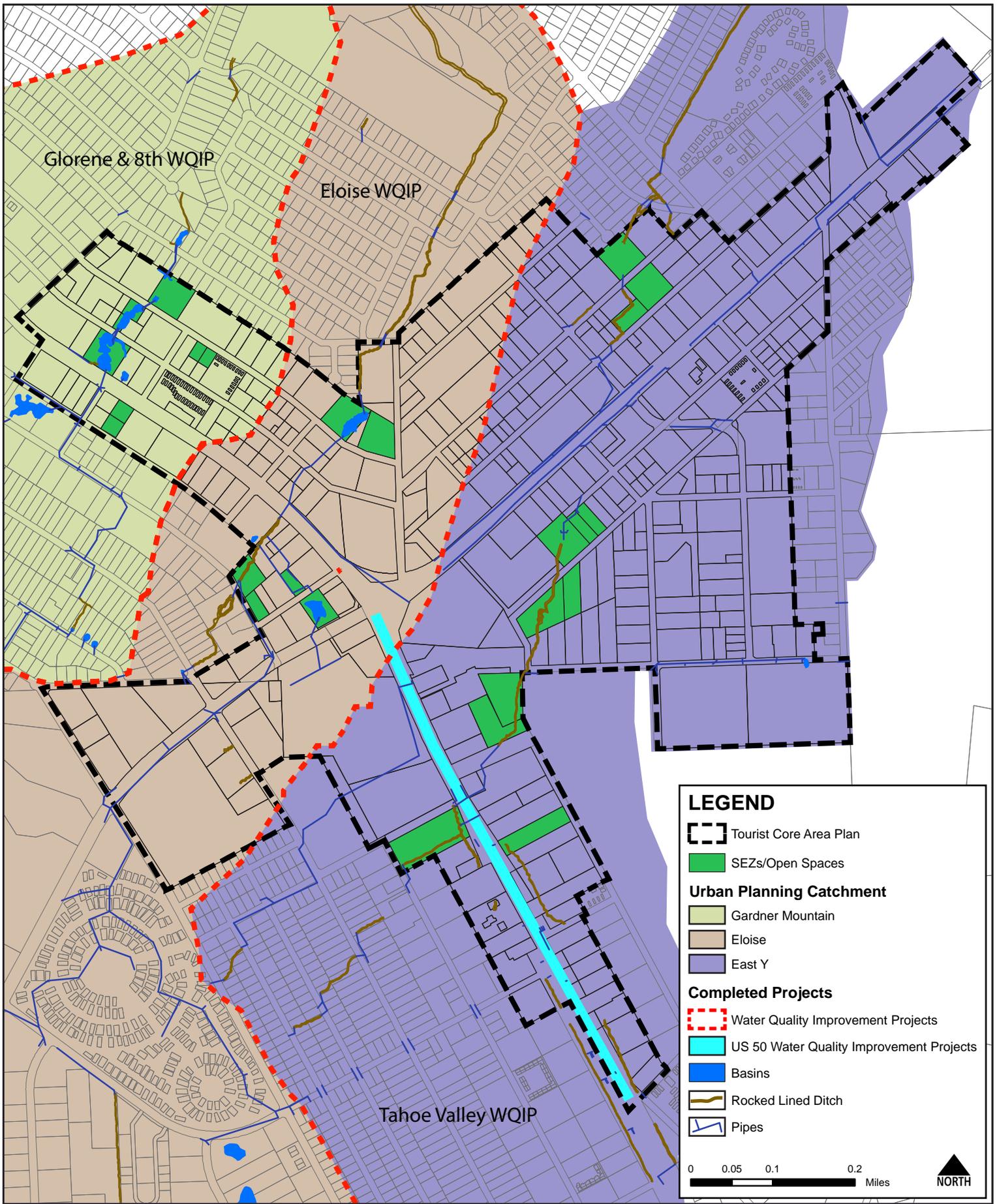
- Tahoe Valley Area Plan Boundary
- Stream Environment Zone (SEZ)

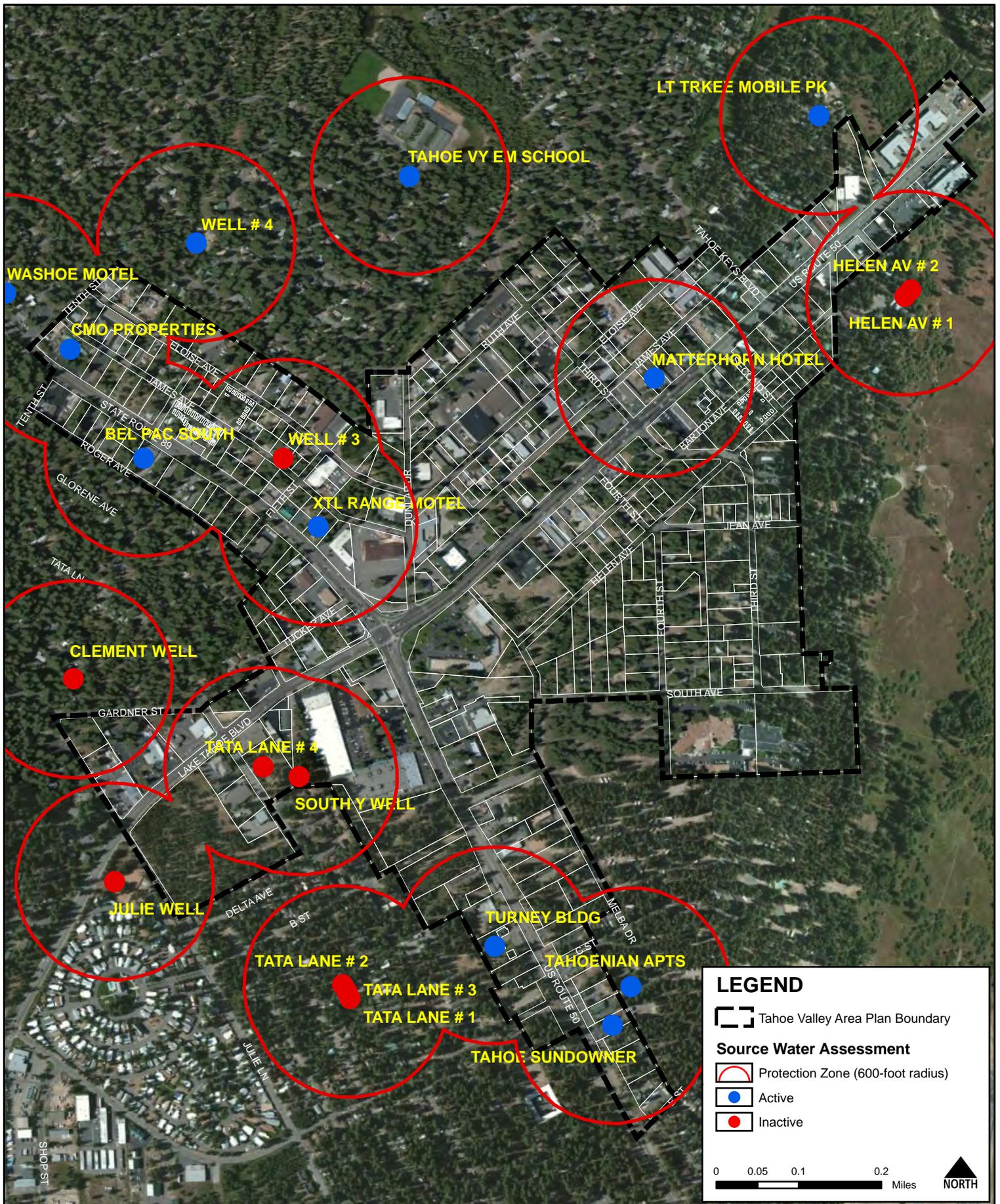
Land Coverage by Type

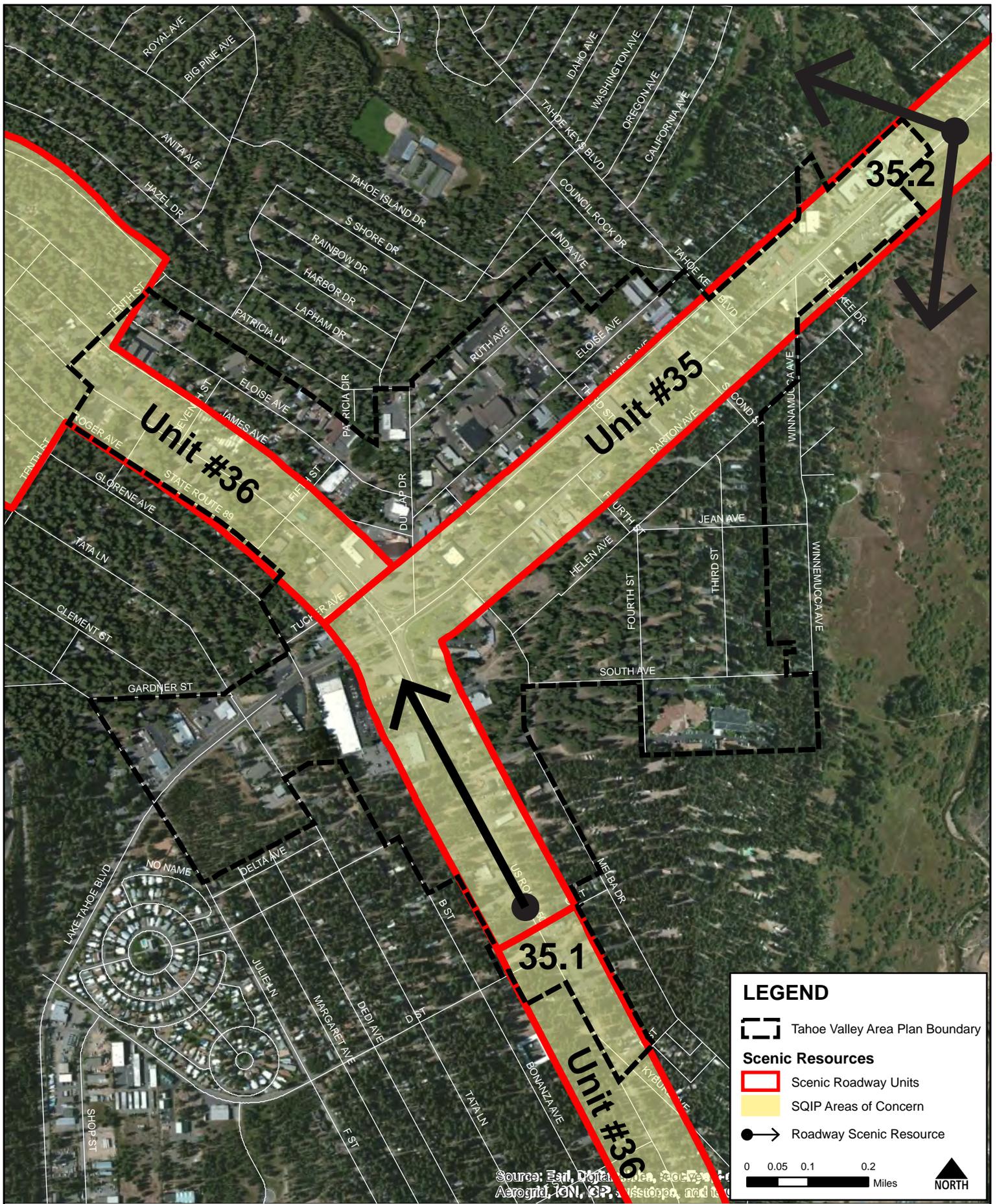
- Trails
- Roads
- Buildings
- Other (e.g., parking lots)

0 0.05 0.1 0.2 Miles NORTH









SECTION 4

Vision and Guiding Principles



The Tahoe Valley Area Plan reflects the City of South Lake Tahoe's effort to rekindle the economic vitality and spirit of the Tahoe Valley community. The Tahoe Valley area is ready to recapture its local prominence, and the Tahoe Valley Area Plan outlines the strategy for the area's resurrection. This section summarizes the community's vision for the future of the Tahoe Valley. It also summarizes the guiding principles that were used to develop the Land Use Zoning Districts and the Plan's goals, objectives, and policies.

4.1 Vision Statement

The City aims to focus the energy of local residents, landowners, developers, and public officials to spark the transformation of the Tahoe Valley area and, in turn, the entire City of South Lake Tahoe. The City intends to use the Tahoe Valley Area Plan to create physical, social, and economic links that will improve the Tahoe Valley area and create a sustainable community. The vision for the Tahoe Valley area is as follows:

"We shall transform the Tahoe Valley area into a strong and vibrant business district, creating a community that includes continuous and complimentary developments. We shall embrace outdoor enthusiasts and provide a healthy balance of commerce, employment, entertainment, and livability. Our Area Plan will create smaller, safer pedestrian-friendly districts that encourage patrons to visit multiple destinations for each vehicle trip. Our plan shall encourage retail businesses and services that meet local needs, encourage local ownership, and appeal to visitors seeking to mix with the local community.

Architectural and aesthetic standards shall emphasize a mountain identity through sensitive building design that incorporates open public spaces, and a walkable environment emphasizing unifying elements. Quality building designs shall emphasize character through massing and articulation that are characteristic of a mountain environment. No blank wall will be displayed. Pedestrians, transit riders, and cyclists will be encouraged and accommodated. Parking areas shall be unobtrusive, and serve more than one property.

We shall re-knit the fragmented elements that isolate the Tahoe Valley neighborhoods through a network of sidewalks, trails, and access to transit facilities. The transition between more intensive uses shall include design elements to ensure compatibility. The plan shall weave active and passive recreational staging areas, connecting to recreational opportunities located outside the plan boundaries. The Transit Center shall play a greater role, to include moving people locally and regionally within and outside of the Tahoe Basin.

Visually prominent and attractive design features shall welcome visitors entering the City from the scenic corridors of US 50 and SR 89. Through the use of environmentally conscious development techniques and restoration activities, the Area Plan will be considered a contributing component of the regional

ecosystem. Private and public development shall contribute to the creation of indoor and outdoor public spaces within these districts.”

4.2 Guiding Principles

As part of the Vision, the following guiding principles create a foundation for a more sustainable, economically prosperous, communal and environmental future for Tahoe Valley:

- *Create a diversity and concentration of resident servicing shops and services that also appeals to the visitor;*
- *Create public plazas, gathering spaces, and active streetscapes to promote a lively market;*
- *Encourage ground floor retail, large display windows, retail, dining and entertainment to activate the pedestrian streetscape;*
- *Preserve or replace trees and use native landscaping;*
- *Design buildings that reflect Tahoe’s mountain identity;*
- *Include transit and alternative travel modes as an essential part of the Tahoe Valley experience;*
- *Connect Tahoe Valley to adjacent residential neighborhoods with paths, trails, and sidewalks;*
- *Located multi-family residential development within close proximity to the Tahoe Valley Town Center;*
- *Develop new programs and new facilities to meet the needs of residents such as indoor and outdoor theaters,*

SECTION 5

Land Use and Community Design



This section supplements the Land Use Element of the TRPA Goals and Policies, and the Land Use Element of the City of South Lake Tahoe General Plan. This Section includes the Area Plan's Goals and Policies related to land use and establishes land use and zoning regulations.

The overarching vision of the 2030 South Lake Tahoe General Plan and the TRPA Regional Plan and Sustainable Communities Strategy is to create a more sustainable community. One way this will be achieved is through focusing new development and revitalization within the areas designated as Town and Regional Centers in the Regional Plan and as Nodes in the General Plan. For the Tahoe Valley area, the focus is commercial, mixed-use, and multi-family development in a pedestrian, bicycle and transit oriented land use pattern that harmonizes with the overall natural environment. The land use and community design policies in this section seek to encourage pedestrian/bicycle-friendly and transit oriented developments that help reduce resident and visitor dependency on automotive travel and building types and patterns that reflect the mountain and surrounding natural environment.

5.1 Land Use Districts

The following describes the six zoning districts and one overlay zone that are included in the Tahoe Valley Area Plan and the land use vision for each district. Figure 11 shows the boundaries and location of each zoning district.

Town Center Gateway District (TC-G)



The Gateway District, which includes properties surrounding the “Y” intersection and properties along Lake Tahoe Boulevard and Emerald Bay Road, is a key arrival area for travelers to South Lake Tahoe.

This district is intended to create an attractive mixed-use commercial corridor that provides a welcoming gateway to South Lake Tahoe and conveys the arrival to South Lake Tahoe as a destination. The physical form varies to reflect the commercial mixed-use character of the gateway corridor and to transition from surrounding, lower-density districts to the Town Center Core. Allowable uses include tourist accommodation, retail, commercial, recreation, office uses, as well as mixed-use residential development.

Town Center Core (TC-C)



This district is the heart of the Tahoe Valley area and intended to become a place of public gathering and retail activity meeting the needs of both area residents and tourists. This district allows for vertical mixed-use projects with a focus on ground-level active storefronts and pedestrian- and transit-oriented

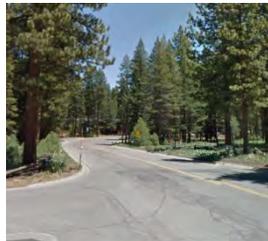
development that encourages pedestrian activity and supports multi-modal transportation. Allowable uses include tourist accommodation, retail, commercial, entertainment, and office uses, as well as mixed-use residential development.

Town Center Mixed-Use Corridor (TC-MUC)



This district is intended to facilitate the transformation of the eastern and western portions of Lake Tahoe Boulevard into a multi-modal, mixed-use corridor. Allowable uses include a rich mixture of retail, service, public facility, recreation, entertainment, and housing organized in a compact development pattern, creating an aesthetically-pleasing and safe environment for pedestrians, cyclists, and automobile drivers.

Town Center Neighborhood Professional (TC-NP)



This district is intended to allow a variety of residential and non-residential uses to encourage a greater mix and intensity of uses in a pedestrian and bikeable environment at a scale and form that is appropriate to its neighborhood context and adjacent residential uses. Allowable uses include commercial, public service, healthcare, and office uses, social services, neighborhood parks, as well as all types of residential development.

Town Center Healthcare District (TC-HC)



The Healthcare District contains the area within the immediate vicinity of Barton Hospital, north and south of South Avenue and between Third Street and Winnemucca Avenue. This district will continue to be the core area for healthcare services and other related supporting services.

Commercial Mixed-Use Service (CMX-S)



This district is intended to provide for a mix of small- and medium-scale commercial, support services, and light industrial uses to meet local and regional demand.

5.2 Goals and Policies

This subsection outlines land use and community design goals and policies for the Tahoe Valley Area Plan. These goals and polices implement the vision and guiding principles discussed earlier.

Goal LU-1: Land Use

To shape the Tahoe Valley area into an attractive pedestrian, bike and transit-oriented mixed-use commercial service district.

Policy LU-1.2 – Active Streets

Animate streets by allowing uses such as retail, entertainment, special events, outdoor dining and seating, oriented towards the sidewalk.

Policy LU-1.3 - Connectivity

Create bike, pedestrian and open space connections from the Tahoe Valley Area Plan to the adjacent residential neighborhoods.

Policy LU-1.4 – Special Events

Designate areas within the Town Center Core as a Special Events Area allowing for temporary events and activities to increase pedestrian and economic activity in these areas.

Policy LU-1.5 – Outdoor Markets

Encourage the operation of Farmers’ and Outdoor Markets in the Town Center Core Special Events Area.

Policy LU-1.6 – Auto-Oriented Commercial Use Consolidation

The City shall encourage the consolidation of existing auto-oriented commercial development into pedestrian-oriented development.

Goal LU-2: Community Services

To transform the Tahoe Valley area into an attractive gateway district that both residents and visitors access to high quality local and regional commercial uses, entertainment opportunities, recreation experiences, and tourist accommodation options.

Policy LU-2.1 – Mixed Uses

Promote mixed land uses with the Tahoe Valley area and adopt a use list that permits a wide range of commercial, retail, entertainment, tourist, residential, recreation, and public uses to service residents and visitors. This can be achieved through vertical or horizontal mix of uses within the same structure or in different structures on the same site.

Goal LU-3: Community Design

To improve community character through appropriate site and building design and promote the creation of an urban neighborhood in South Lake Tahoe.

Policy LU-3.1 – Distinctive Design

Create connected, and walkable districts that have a strong sense of identity while avoiding

repetition in building design and siting.

Policy LU-3.2 – Architectural Style

Promote the use of “mountain” architectural theme in new development and through remodeling. Mountain architecture features the use of pitched roofs, natural colors, and natural material such as rock and wood.

Policy LU-3.3 – Inter-connected Development

Ensure that every project is planned to enhance the physical, visual, and social connections to surrounding parcels and to the larger community.

Policy LU-3.4 – Visual Impact of Parking

Minimize the visual impact of parking by locating it to the rear and/or side of buildings and providing landscape screening.

Policy LU-3.5 – Public Art

Integrate public art into streetscape and landscape areas along US 50 and SR 89, and in the Town Center Core District. Encourage murals in the Commercial Mixed-Use Service District. All proposed public art must be approved by the City of South Lake Tahoe Public Art Review Process.

Policy LU-3.6 – Lighting

Establish pedestrian-scaled and strategically-placed lighting along US 50, SR 89, and Lake Tahoe Boulevard that promotes pedestrian safety and comfort and enhances architectural and site design. Prevent unnecessary and intrusive lighting that detracts from the beauty and view of the night sky.

Policy LU-3.7 – Building Rehabilitation

Encourage redevelopment through demolition, reconstruction, rehabilitation, remodeling, or other aesthetic improvement of buildings and signage.

Goal LU-4 Revitalization and Redevelopment

To create a sustainable economic and community activity center through revitalization, reuse, and development of existing and buildable vacant sites in the Tahoe Valley Area.

Policy LU-4.1 – Infill and Underutilized Site Development

Encourage physical change and economic development within the Tahoe Valley area by promoting infill development on high capability lands and redevelopment and revitalization on underutilized and over covered lands with existing infrastructure.

Goal LU-5 Town Center Core

To shape the Town Center Core into a primary area in the City for a vibrant mixed-use, resident-serving commercial and residential node.

Policy LU-5.1 – Town Center Core

Create a compact, mixed-use, vibrant Town Center Core district focused on the “Y” intersection that includes resident-serving commercial uses, retail and entertainment uses, and workforce and affordable housing.

Policy LU-5.2 – Commercial Activity

Create a pedestrian-oriented land use pattern that is served by transit and alternative transportation options. The Town Center Core should be a unique and recognizable district in South Lake Tahoe with its pedestrian scale mountain architecture, active streetscapes, mix of uses, and focus on resident service commercial uses.

Policy LU-5.3 – Commercial Activity

Facilitate the development of commercial projects that include a mix of businesses that will kick-off revitalization of the Tahoe Valley area. The first priority is the Town Center Core.

Policy LU-5.4 – Gathering Spaces

Create gathering spaces in the Town Center Core to promote social interaction, cultural and civic uses. The spaces should provide an area for people to gather, interact, relax, reflect, and pass time.

Policy LU-5.4 – Entertainment

Promote the establishment of the Town Center Core District as a central entertainment and recreation attraction for residents and visitors. Promote uses such as movie theaters, skating rinks, and recreation based retail uses.

Policy LU-5.4 – Building Orientation

Orient buildings directly onto the four corners of the “Y” intersection and adjacent plazas so their entrances face and have frontage to the intersection with parking behind. Where there is adjacent open space, recreation, and/or pedestrian corridors, buildings may orient entrances in that direction but shall not be designed with an inactive “backside” facing the street.

Policy LU-5.5 – Viewshed

Maximize views of the surrounding trees and mountains from the “Y” intersection through appropriate site and building design.

Policy LU-5.6 – Neighborhood Compatibility

Require taller buildings to be designed to minimize negative visual impacts, particularly on adjacent residential areas, through such techniques as facade articulation, building orientation and location, and stepping back upper floors above two stories.

Policy LU-5.7 – Multi-Family Housing

Prohibit multi-family housing from fronting on either Emerald Bay Road or Lake Tahoe Boulevard within the Town Center Core District (TC-C).

Goal LU-6: Neighborhood Professional District

To provide an area for professional offices, social services, and health-related facilities that can support and complement Barton Hospital.

Policy LU-6.1 – Healthcare

Encourage the concentration of healthcare providers and services in the Neighborhood Professional District (TC-NP) to provide convenience to patrons. This area will provide a mix of residential, tourist, and healthcare uses within a cohesive neighborhood setting that encourages walking and biking.

Policy LU-6.2 – Barton Hospital Campus

Encourage Barton Healthcare to consolidate its facilities within the Hospital Campus District (TC-HC). This district would provide for healthcare services such as an orthopedic center, employee housing, temporary housing for patients’ family members, and a parking garage.

Policy LU-6.3 – Barton Hospital Campus Master Plan

Encourage Barton Healthcare to prepare a master plan for the Healthcare District that provides a blueprint for future development and redevelopment that connects existing and future facilities through an integrated pedestrian and bike friendly system. With an adopted Master Plan, allow facilities located within the Healthcare District associated with Barton Healthcare to be treated as a public service use.

Policy LU-6.4 – Parking

The master plan should include an analysis of parking needs in the Healthcare District and incorporate parking management strategies to centralize and consolidate parking to create a “park-once” environment that is efficient, convenient and accessible by all the facilities located within the district.

Goal LU-7: Commercial Mixed-Use Service District

To maintain an area for small-scale industrial and commercial mixed uses in the South Shore community.

Policy LU-7.1 – Industrial Uses

Support the industrial needs of South Shore’s full service community by continuing the secondary industrial area.

Policy LU 7.2 – Storage Containers

Require industrial-style storage containers for on-site storage to be enclosed and screened from the public right-of-way, bike paths, lanes and routes, pedestrian trails and sidewalks, and adjacent open spaces.

Goal LU-8: Sustainability

To create a sustainable community with reduced private automotive trips, reduced greenhouse gas emissions and a more energy-efficient built environment.

Policy LU-8.1: Sustainable Communities Strategy

Implement the Tahoe Metropolitan Planning Organization Sustainable Community Strategy in the Tahoe Valley area by integrating land use, transportation, and housing strategies to create walkable town centers, provide alternative transportation options, provide for a variety of housing types in close proximity to services, connect neighborhoods, and protect the area’s natural resources.

Policy LU-8.2: Commercial Floor Area Incentives

Provide incentives such as streamlined permitting and priority access to Commercial Floor Area and Residential Allocations for projects that incorporate green building measures. Also see Policy ED-1.3.

Goal LU-9: Transfer of Development

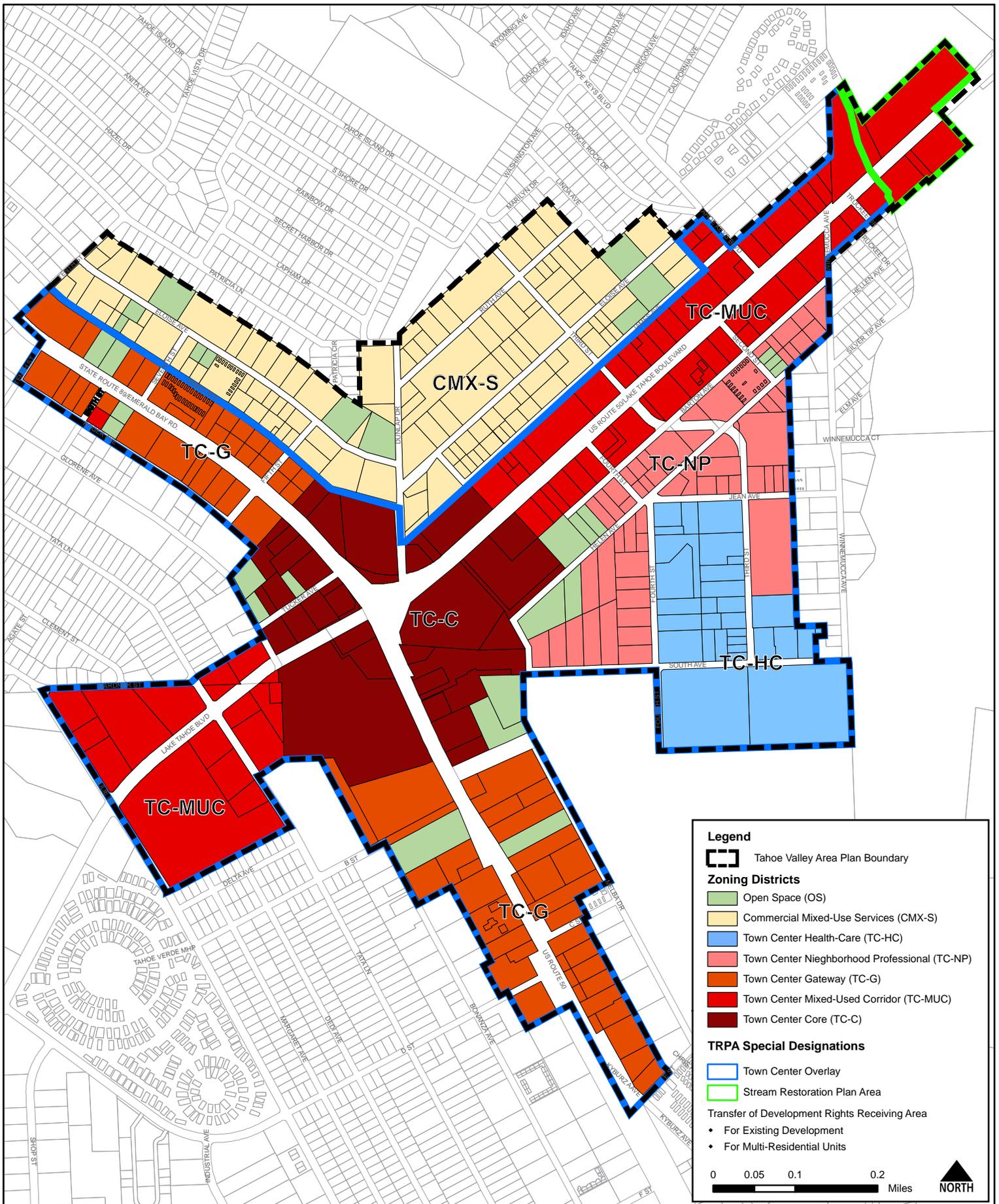
To focus development in centers in order to maximize incentives and create transit-, bicycle-, and pedestrian-oriented places that serves the needs of both residents and visitors.

Policy LU-9.1

Encourage and allow for the revitalization and consolidation of development within centers by allowing for the transfer in of residential units of use and tourist accommodation units that have been converted to commercial floor area pursuant to TRPA Code Section 50.10.

5.3 Development and Design Standards

The Development and Design Standards and permissible use list for the Tahoe Valley Area Plan are included in Appendix C.



SECTION 6

Economic Development

The Tahoe Valley area is largely dominated by obsolete strip commercial development along South Lake Tahoe's key corridor (US 50). This type of commercial development does not create an attractive environment for achieving the City's long-term economic development goals. Shortcomings include the area's multiple curb cuts, relatively small and fragmented parcels, stand-alone structures, older and sometimes obsolete motels struggling to survive, and lack of coordinated sidewalks, bike paths, landscaping, and other amenities. There is a clear need to revitalize the area to increase economic activity. The area is in a unique position to capture more local-resident-serving commercial sales, as well as capture more tourist sales. This local market includes general merchandise, apparel, home furnishings, and appliances, and building material groups which primarily (but not exclusively) serve local residents. This subsection outlines economic development goals and policies for the Tahoe Valley Area Plan.

6.1 Goals and Policies

Goal ED-1: Economic Development

To create a commercial, office, residential, and civic hub that will be a community focal point and destination place for South Lake Tahoe residents and visitors.

Policy ED-1.1 – Gateway District

Provide incentives and remove barriers to help redevelop the Gateway District into a mixed-use office, commercial, and residential corridor.

Policy ED-1.2 – Town Center Core

Provide incentives and remove barriers to help redevelop the Town Center Core District into the key focal place of South Lake Tahoe. Focus uses toward commercial retail, dining, groceries, professional offices and outdoor recreational uses

Policy ED-1.3 – Commercial Floor Area Incentives

Coordinate with TRPA to encourage the conversion of units of use to commercial floor area and promote their transfer to the Tahoe Valley Area Plan. Projects that meet the City's economic and environmental goals, and implement one of the following community benefits may earn bonus CFA, subject to availability, from the City's CFA pool. CFA allocation from the City's pool is at the discretion of the City Council based on the following criteria for eligibility:

- A. Achieve a third party green building certification such as LEED, Energy Star or Green Point Rated;
- B. Incorporate housing units in the project;
- C. Incorporate mix uses into the project with commercial, residential and office uses;
- D. Transfer converted CFA from outside of the Area Plan boundary to a project inside the

Area Plan boundary;

- E. Participate in the implementation of the Tahoe Valley Greenbelt; or
- F. Develop public facilities such as mini neighborhood parks in the adjacent residential areas.

Policy ED-1.4 – Coverage Incentives

Promote revitalization in the Tahoe Valley area by leveraging the City’s SEZ restoration credits and banked land coverage to help private projects offset their excess coverage mitigation fees and provide a source for land coverage transfers. See Policy NCR X-XX

Goal ED-2: Neighborhood Professional and Healthcare Districts

To support and expand South Lake Tahoe’s healthcare industry.

Policy ED-2.1 – Healthcare

Encourage additional healthcare-related facilities to locate in the Healthcare District.

Policy ED-2.1 – Barton Hospital Campus Master Plan

Encourage and collaborate with Barton Hospital to develop a Campus Master Plan for the Healthcare District that promotes connectivity, walkability, and bikeability.

Policy ED-2.2 – Barton Hospital Incentives

Provide flexibility for the Barton Hospital to engage in public-private ventures. Upon adoption of a Master Plan, healthcare services in which Barton Hospital owns a majority interest and located within the Healthcare District will be considered public services provided by a non-profit organization, and therefore will be exempt from the requirements for CFA entitlements.

Goal ED-3: Commercial Mixed-Use Service District

To modernize the existing industrial uses in the Commercial Mixed-Use Service District.

Policy ED-3.1 – Retention

Continue to maintain industrial uses in the Commercial Mixed-Use Service District and allow residential uses if they are part of a mixed-use project.

Policy ED-3.2 – Modernization

Support the modernization of the existing industrial uses in the Commercial Mixed-Use Service District so they are less polluting, less obtrusive to neighboring uses, and consume less energy.

Policy ED-3.3 – South Tahoe Refuse Relocation

Assist South Lake Tahoe Refuse to upgrade their existing facilities with state-of-the-art improvements.

SECTION 7

Housing

This section supplements the Housing Sub-Element of the TRPA Regional Plan Goals and Policies, and the Housing Element of the City of South Lake Tahoe General Plan. This section provides the policy context for the Tahoe Valley Area Plan to achieve the vision of creating a range of housing opportunities and choices. These opportunities include increasing the availability of quality workforce and affordable housing, providing for housing opportunities within close proximity to the Tahoe Valley Town Center Core, and allowing for the mixing of land uses to encourage live/work opportunities that allow business to grow and evolve over time.



7.1 Goals and Policies

Goal H-1: Housing Development

Promote the construction of a variety of high quality housing types for all income levels in the Tahoe Valley area.

Policy H-1.1 – Housing Options

Provide for a range of housing options that include affordable, moderate-income, and workforce housing so that people can live and work in the City.

Policy H-1.2 – Housing Incentives

Provide the following incentives to encourage the development of a range of housing options in the Tahoe Valley planning area:

- 70 percent land coverage in the TRPA designated Town Center
- 90 percent land coverage in the Town Center Core District
- A maximum of 25 dwelling units an acre in the center
- Height up to 45 feet in the Town Center Core District and 42 feet elsewhere
- Access to the City's banked coverage pool to offset excessive coverage mitigation fee and for transfer of coverage (see Policy NCR-7.2)
- Secondary units allowed on parcels less than an acre.

Policy H-1.3 – Residential Improvement

New residential development may take the form of both infill of vacant parcels or reuse of existing developed parcels that benefit from "grandfathered" excess land coverage, existing sewer units, and other benefits associated with the redevelopment of existing lots.

Policy H-1.4 – Compatibility with Neighboring Uses

Ensure new and rehabilitated residential developments are designed to be compatible with neighboring uses.

Policy H-1.5 – Redirection of Development

Direct high density residential development to areas of Tahoe Valley that are within walking distance of public transit and services.

Policy H-1.6 – Commercial Mixed-Use Service District

Allow residential uses within the Commercial Mixed-Use District if they are part of a mixed-use project.

Policy H-17 – Tahoe Livable Communities Program

Collaborate and support efforts of the California Tahoe Conservancy to remove blighted properties and repurpose associated development commodities. Encourage the use of residential commodities for affordable housing in Tahoe Valley area.

Policy H-1.8 – Regional Housing Needs Assessment

Collaborate with TRPA on the Regional Housing Needs Assessment to develop updated policies and implementation strategies to remove barriers and facilitate the development of affordable housing. Regional housing policies and programs should complement and not conflict with or duplicate state or local housing policies and programs.

Policy H-1.9 – Housing Needs

The City should encourage the consolidation of parcels within the Tahoe Valley area or the incorporation of parcels adjacent to the Area Plan boundaries for the development of multi-residential units.

Policy H-1.10 – Conversions

Encourage property owners of older motels units located in the Gateway District to convert tourist accommodation units to residential units of use onsite pursuant to TRPA Code Section 50.10.2.

Goal H-2: Mixed-Use Housing

To facilitate the development of housing for all income levels in association with mixed-use development within the Tahoe Valley Area Plan.

Policy H-2.1 – General

Encourage the creation of residential uses to support and add vitality to commercial and public service projects in the Tahoe Valley area.

Policy H-2.2 – Town Center Core District

Allow residential uses in the Town Center Core District only when mixed with commercial, recreation, entertainment or tourist primary uses. Residential uses shall be located above ground floor commercial use or in separate residential structures located away from arterial roadways.

Goal H-3: Energy Efficiencies

To ensure energy efficiencies and appropriate weatherization for all new and existing homes.

Policy H-3.1 – New Construction

Ensure that new construction meets the Title 24 energy conservation requirements of the State of California.

Policy H-3.2 – Green Building

The City shall encourage and promote green building practices during the development review process and implement the City’s Green Building Incentive Program.

SECTION 8

Transportation and Circulation



The Transportation and Circulation Section is a supplement to the Transportation and Circulation Element of the City of South Lake Tahoe General Plan, the TRPA/TMPO Regional Transportation Plan and Sustainable Communities Strategy Plan. Consistent with these plans, this section provides the policy context for the Tahoe Valley Area Plan to achieve its vision for future transportation and circulation systems which are critical to the community's livability, economic viability, and environmental quality.

This section contains goals and policies and implementation strategies that will help create a safe, efficient, and integrated transportation system that provides quality mobility options and connectivity for all sectors of the population. The Plan seeks to improve connections to adjacent neighborhoods, increase pedestrian and bicycle connections, improve transit access, conserve energy resources, reduce vehicle miles traveled, reduce greenhouse gas emissions, improve air quality and connect Tahoe Valley to the regional transportation system.

8.1 Goals and Policies

Goal T-1: Roadways

To provide a safe and efficient vehicular circulation for the Tahoe Valley Area.

Policy T-1.1 – Level of Service

Strive to maintain a level of service (LOS) D or better on all arterial and collector roads, and signalized intersections. This LOS standard may be exceeded during peak periods, not to exceed 4 hours per day when provisions for multi-modal amenities and/or services (such as transit, bicycling, and walking facilities) are adequate to provide mobility for users.

Policy T-1.2 – “Way-Finding” Signage

Install an integrated way-finding and directional signage program through the Tahoe Valley area directing visitors on foot, bikes, or in vehicles, to recreation, commercial, tourist, transit and parking facilities.

Policy T-1.3 – US 50 and SR 89

Coordinate with Caltrans to ensure that the US 50 and SR 89 water quality improvement projects include the installation of bike lanes, pedestrian sidewalks, sidewalk lighting, and curb and gutter to convey water for treatment.

Policy T-1.4 – Truck Routes

Minimize truck traffic through residential neighborhoods by requiring trucks to use designated and posted truck routes.

Policy T-1.5 – Vehicle Short Cuts

Minimize vehicular traffic “shortcuts” through residential areas and commercial parking lots through use of street design, traffic controls, and traffic calming measures.

Policy T-1.6 – Traffic Movements on Lake Tahoe Boulevard

Discourage left turn movements on Lake Tahoe Boulevard, with the exception of signalized intersections.

Policy T-1.7 – Complete Streets

Develop and adopt complete street roadway design standards for US 50 and the following collector streets: Third Avenue, South Avenue and Tahoe Keys Boulevard.

Policy T-1.8 – Healthcare District

Coordinate with the Barton Healthcare on the Campus Master Plan to ensure that any proposed roadway modifications or abandonment in the Healthcare District meet standards for safety, traffic, bike and pedestrian movement.

Goal T-2: Parking and Access

To create an accessible, efficient, and an aesthetically-pleasing parking system in the Tahoe Valley Area.

Policy T-2.2 – Shared Parking and Driveway Consolidation

Encourage shared-use parking facilities and agreements between businesses/uses on adjacent parcels.

Policy T-2.3 – Parking Structures

Encourage the development of above-ground or underground structured parking, or public/shared parking lots in the Town Center Core to serve surrounding uses at the “Y” intersection.

Policy T-2.4 – Driveway Consolidation

Where feasible, reduce the number of ingress/egress points along US 50 and SR 89 by combining and realigning driveways to improve traffic flow and eliminate pedestrian and bicycle conflicts with vehicles.

Policy T-2.5 – New Project Access

Require new projects located on corner parcels to provide their vehicle access from side streets instead of from Lake Tahoe Boulevard or Emerald Bay Road, unless it’s shown that highway access would be safer.

Policy T-2.6 – Mixed-Use Project Parking

Allow mixed-Use projects to reduce their parking requirements provided that a parking demand analysis is submitted and approved by the City.

Goal T-3: Pedestrian Facilities

To create a functional, safe, convenient, and integrated pedestrian system in the Tahoe Valley area and the surrounding neighborhoods.

Policy T-3.1 – Connectivity

Provide adequate pedestrian and bicycle facilities such as continuous sidewalks, bike paths and bike lanes throughout the Tahoe Valley area that connect commercial, health services, entertainment, residential, and recreation areas.

Policy T-3.1 – Pedestrian Priority

Give pedestrian safety and convenience highest priority in site planning and roadway design.

Policy T-3.3 – Pedestrian/Automobile Buffer

Develop landscape strips between sidewalks and arterial roadways to buffer pedestrians from vehicular traffic.

Policy T-3.4 – Sidewalks

Require all projects that include new development, major remodeling, and any significant addition, located on an arterial or collector street to provide and maintain sidewalks along the public street frontage. Such sidewalks shall comply with ADA requirements.

Policy T-3.5 – Pedestrian Obstacles

Remove and/or relocate any existing impediments to pedestrian movement along sidewalks, such as poles, trees, signs, wires, and other public/private utility infrastructure.

Policy T-3.6 – Pedestrian/Bicycle Crossing at the “Y” Intersection and US 50

Develop strong pedestrian and bicycle links (e.g., crosswalks and refuge islands) between the four corners of the “Y” intersection and along US 50. Coordinate with Caltrans to evaluate alternative crossings that create safe passage across US 50 for pedestrians and cyclist.

Policy T-3.8 – Pedestrian Access

Develop a strong pedestrian (sidewalks, paths and trails) access system that effectively links adjacent residential neighborhoods to Tahoe Valley’s employment centers, commercial uses, public service uses, and recreational and educational opportunities as an alternative to vehicle trips.

Goal T-4: Bikeways

To develop a safe and efficient bikeway system that promotes biking as an alternative to local vehicle trips.

Policy T-4.1 – Bikeway System

Develop a bikeway system that effectively links residential neighborhoods, employment centers, commercial areas, public uses, and recreational and educational centers, both within and outside of the Tahoe Valley area. Bicycle and pedestrian improvements that are identified in the City General Plan, TRPA Regional Plan, and/or TRPA’s Lake Tahoe Regional Bike and Pedestrian Master Plan, which are adjacent to or within private development, shall be implemented as conditions of approval.

Policy T-4.2 – Bikeway Linkages

Integrate and link existing bike paths with regional paths. Connect the Tahoe Valley trail system to the proposed Meyers – Stateline Greenway bike trail.

Policy T-4.2– Winter Use

Designate, where appropriate, Class I bike trails for winter use. Consider plowing the trails where pedestrians need access, or designate for cross country or snowshoe use. Implement snow removal practices along roadways to ensure that bike trail crossings are not blocked by berms.

Policy T-4.3 – Bicycle Storage

Install bicycle racks or lockers at all public parks, gathering areas, and the transit center, and require them in all new private construction developments or remodels that require major design review.

Policy T-4.4 – Bike Route Relocation

Explore bike route alternatives to the Eloise Ave route that run outside the Industrial District in order to provide a more appealing bicycling experience. Possible routes for consideration include Council Rock Drive and Washington Avenue.

Policy T-4.5 Bicycle Route maintenance

Prioritize the maintenance of roads containing designated bicycle routes.

Policy T-4.5 –Safe Routes to School

Coordinate with the South Lake Tahoe Unified School District, non-profit community groups, and the Tahoe Transportation District to develop a Safe Routes to School Master Plan. Designate Washington Avenue and Third Street as potential safe routes to school.

Policy T-4.6 –South “Y” Intersection Enhancements

Coordinate with Caltrans to enhance bicycle safety at the South “Y” intersection by installing combined bike lane/turn lane, intersection crossing markings, and colored pavement to clearly demark auto and bicycling travel lanes.

Goal T-5: Transit

To promote the use and expansion of transit options for South Lake Tahoe citizens.

Policy T-5.1 – Transit Services

Work with the Tahoe Transportation District to ensure that the Tahoe Valley area and its major employment centers, shopping destinations, and residential neighborhoods are served by frequent bus service.

Policy T-5.2 – Bus Turnouts and Shelters

Coordinate with the Tahoe Transportation District to ensure that bus shelters and bus pullouts are installed at all bus stops throughout the Tahoe Valley area.

Policy T-5.3 – Transit Stops

Require all mixed-use residential/retail site development projects of more than three acres and containing more than five residential units to designate and reserve locations for transit/school bus stop locations.

Policy T-5.5 – Transit Center

Work with Tahoe Transportation District, and Amtrak to ensure that the City-owned Transit

Center has the capacity for long-term transit growth.

Policy T-5.6 – West Shore Shuttle Service

Work with the Tahoe Transportation District to ensure continual and frequent shuttle service trips during the summer months from the South Y Transit Center to Camp Richardson, the US Forest Service beaches along SR 89, and Emerald Bay.

Policy T-5.7 – Intelligent Signage

Coordinate with Caltrans and California Highway Patrol to install electronic message signs in the Tourist Center Core District to convey traffic and parking conditions at recreation facilities located on SR 89 between the Y intersection and Emerald Bay.

Goal T-6: Sustainable Communities Strategy

To provide a transportation and circulation system that promotes reduction in greenhouse gases, creation of multimodal networks and promotion of a mix of land uses.

Policy T-6.1 – SCS Implementation

Implement the TMPO Sustainable Community Strategy in the Tahoe Valley Area Plan by integrating land use, transportation, and housing strategies to create a walkable Town Center, provide alternative transportation options, provide for a variety of housing types in close proximity to services, connect neighborhoods, and protect the area’s natural resources.

8.2 Implementation Strategies

This subsection outlines transportation and circulation improvements that implement the vision, guiding principles and the goals and policies of the Tahoe Valley Area Plan.

A variety of improvements to the area’s transportation and mobility system intended to encourage safety, connectivity, and access to the area’s surrounding recreational opportunities are proposed by various agencies and are in various phases of design, permitting or environmental review.

CITY OF SOUTH LAKE TAHOE

The following projects and programs are designed to implement the Tahoe Valley Area Plan:

- The Tahoe Valley Greenbelt: The project focuses on two key elements: (1) the construction of a comprehensive regional storm water treatment system (see Tahoe Valley Water Quality Improvement Project description in Section 10, page 77); (2) Improvement of the bicycle and pedestrian transportation system; and (3) construction of a recreation and open space community amenity southeast of the “Y” intersection. The project will include SEZ restoration, installation of pedestrian-bicycle pathways, pedestrian amenities and interpretive signage along with storm water improvements. The Greenbelt will serve as a visual amenity for adjacent residential and commercial uses and provide a bicycle and pedestrian link between the residential neighborhoods and the Tahoe Valley commercial core. There are existing bicycle paths and stormwater facilities in the area, which would be upgraded and enhanced as part of this project.

- Fixin' Highway 50 Initiative: The goal of the initiative is to leverage resources to construct improvements adjacent to Highway 50 at the same time as Caltrans is improving Highway 50.
- Bicycle Transportation Signage System. The City will coordinate with the Lake Tahoe Bicycle Coalition to develop and implement a consistent, functional bicycle transportation signage system that serves the commuting needs of employees, students, business owners, shoppers and tourists in the South Shore. The intent of the program is to implement a consistent way-finding signage system that can direct cyclists and pedestrian traffic to services, places of employment, and recreation opportunities in a safe and efficient manner.
- Industrial area Alternative Bicycle Route Alignments. The City has hired a transportation consultant to explore alternative bike route to the Eloise Avenue bike route that runs through the industrial district in order to provide a more appealing bicycling experience. The consultant will analyze the alternative routes for feasibility as well as safety.
- Safe Routes to Schools. The City is coordinating with the South Lake Tahoe Unified School District, the non-profit Community Mobility Group, and the Tahoe Transportation District to pursue grant funding to develop a Safe Routes to School Master Plan for the four elementary schools located in the south shore. Currently, a \$10,000 On Our Way grant has been awarded to the Community Mobility Group to conduct site observation and audits to support the submittal of a SRTS planning grant to the State of California. The SRTS plan is relevant to Tahoe Valley because of the proximity of Tahoe Valley Elementary School to the planning area and the designation of Third Street and Washington as SRTS routes.

CALIFORNIA TAHOE CONSERVANCY

- South Tahoe Greenway: The proposed South Tahoe Greenway Shared Use Trail will create the backbone of the bicycle and pedestrian network in South Lake Tahoe and will take a major step forward in furthering sustainability goals of the south shore communities. The Greenway will connect residents and visitors to community and recreation destinations from Myers to the Stateline, providing a high quality alternative to private automobile use. The highest priority 1.8-mile segment will link urban neighborhoods in the core of South Lake Tahoe to shopping, social services, schools, and recreation destinations. Continuing the trail system northward to the state line provides important connections to the largest concentration of visitor services in the region, including shopping, lodging and transit. Completion of this link also connects the recreation resources at Van Sickle Bi-State Park to the network. A southern trail extension to Meyers, not part of the current project, would complete the transportation link. A trail from the Tahoe Valley area connecting to the Greenway will provide a link between this area and the regional network.

CALTRANS

- SR 89 "Y" to Cascade Road Water Quality Improvement Project. As part of this water quality improvement project, Caltrans will install a 4-foot shoulder for a Class II bike lane. The project has received a conditional construction permit from TRPA and construction is expected to begin in the Fall of 2014 and be completed by October 2015.

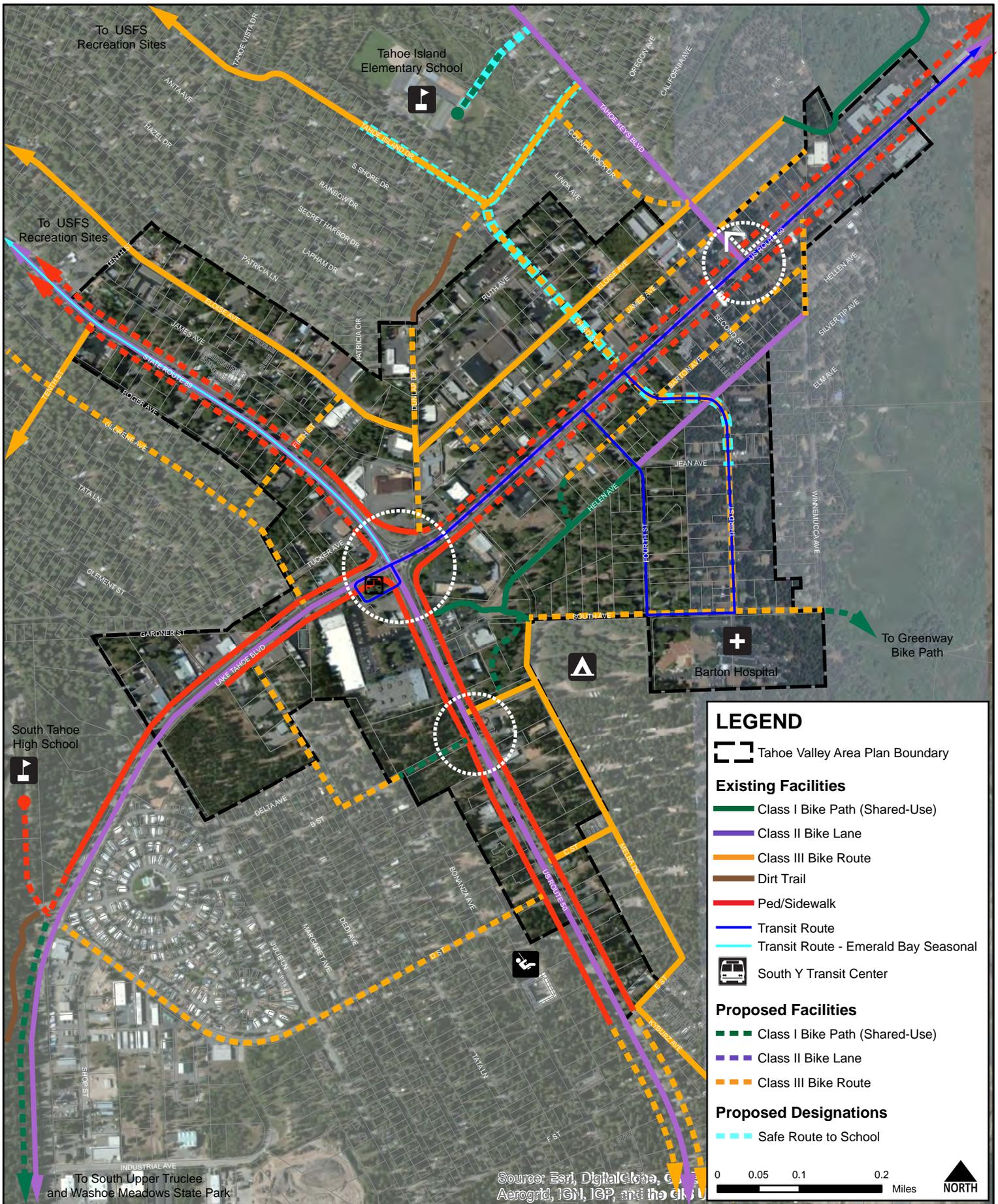
- US 50 “Y” to Trout Creek Water Quality Improvement Project. As part of this water quality improvement project, Caltrans will install 6-foot shoulders for Class II bike lanes, and replace traffic signals, curbs, gutters and sidewalks. Project construction is expected to begin late 2016 and be completed by 2019.
- South “Y” Intersection Bike Lane Stripping. Coordinate with Caltrans to consider upgrading the current bike lane stripping at the South “Y” intersection to install a combined bike lane/turn lane, intersection crossing markings, and colored pavement to enhance public safety and clearly demark auto and bicycling travel lanes.

TAHOE TRANSPORTATION DISTRICT

- South Lake Tahoe Bus Shelters. The Tahoe Transportation District is continuing a program of constructing new shelters in the South Shore. The structures will enhance rider comfort and safety while adding visibility to encourage transit use.
- Transit Service to Recreation Sites. Coordinate with the Tahoe Transportation District to ensure continued and frequent transit service from the Tahoe Valley transit center to the recreation facilities located between Tahoe Valley and Emerald Bay State Park.

Future improvements to the area’s transportation and circulation system will provide opportunities for residents and visitors to walk, bike or use public transit throughout the area. While slight adjustments to the existing roadway system may be possible, the greatest potential for improving the overall transportation and circulation system will be achieved through the development of a comprehensive multi-modal network. Continuing to make connections between the completed trails and sidewalks and proposed additions as well as strengthening the connections to public transportation options will create the foundation for a transportation network with the ability to improve TRPA’s thresholds for Vehicles Miles Travelled (VMT), air quality, water quality and noise, support the local economy, improve public health and support a connected, walkable and bikeable community.

Figure 12 depicts proposed improvements to the transportation network.



SECTION 9

Public/Quasi-Public Facilities and Services



The Public/Quasi-Public Facilities and Services Section is a supplement to the Public/Quasi-Public Facilities and Services Element of the City of South Lake Tahoe General Plan and the Public Services and Facilities Element of the TRPA Regional Plan. This Section provides the policy context for future public and quasi-public facilities and services within the Tahoe Valley area.

9.1 Goals and Policies

This subsection outlines public/quasi-public facilities and services goals and policies for the Tahoe Valley area. These goals and policies implement the vision and guiding principles discussed earlier.

Goal PS-1: Public Facilities

To improve public facilities in the Tahoe Valley area.

Policy PS-1.1 – Co-Locate Public Facilities

Locate appropriate public service facilities in commercial areas so as to add pedestrian/bicycle traffic and serve as a catalyst for private development, provided they do not detract from commercial activity in those areas.

Goal PS-2: Water and Wastewater

To maintain adequate water and sewer service for all properties within the Tahoe Valley area.

Policy PS-2.1 – Lukins Brothers Water

Coordinate with South Tahoe Public Utility District and Lukins Brothers Water in upgrading water system in the Tahoe Valley area.

Goal PS-3: Power

To promote the use of alternative energy to power structures in the Tahoe Valley area.

Policy PS-3.1 – Solar Energy

Encourage new and remodeled structures to design for peak solar orientation, include solar panels, and incorporate design techniques to reduce energy consumption.

Goal PS-4: Broadband

To promote economic prosperity by providing digital infrastructure in the Tahoe Valley area.

Policy PS 4.1 – Fiber Optic

Coordinate with the business community, Caltrans, and other public service and facility providers to plan for and construct fiber-optic conduit in the Tahoe Valley area.

SECTION 10

Natural and Cultural Resources



The Natural and Cultural Resources Section is a supplement to the Natural and Cultural Resources Element of the City of South Lake Tahoe General Plan and to the Conservation Element of the TRPA Regional Plan. Consistent with these plans, this section provides the policy context for the Tahoe Valley Area Plan to achieve its vision for future natural and cultural resource protection.

The City's many resources contribute to its economy and are important elements in the quality of life of South Lake Tahoe residents and quality of the recreation experience for visitors. The preservation of these resources is essential for a prosperous and sustainable future. This section contains goals and policies and implementation strategies that focus on the preservation and enhancement of scenic, water quality, air quality, soil conservation, and stream environment zones.

10.1 Goals and Policies

This subsection outlines natural and cultural resource goals and policies for the Tahoe Valley Area Plan. These goals and policies implement the vision and guiding principles discussed earlier.

Goal NCR-1: Natural Resources

To design neighborhoods, sites, and buildings that complement the natural environment.

Policy NCR-1.1 – TRPA Coordination

Coordinate with TRPA on annual monitoring, reporting, and recertification of the Tahoe Valley Area Plan to ensure that projects permitted by the City are consistent with the goals, policies, and standards of the Tahoe Valley Area Plan, the Regional Plan, and implements the Sustainable Communities Strategies.

Goal NCR-2: Energy Conservation

To improve design and construction practices so that new buildings last longer, cost less to operate, and contribute to increased productivity and provide better working environments for workers and residents.

Policy NCR-2.1 – Green Buildings

Ensure that the design, construction, and operation of new construction and major renovations are consistent with the California Green Building Code and other state standards for energy efficiency. Promote and encourage property owners to use incentives provided for in the City's Green Building Incentive Program.

Goal NCR-3: Scenic Resources

To achieve threshold attainment for Scenic Roadway Unit #1, Unit #35, and Unit #36A within the Tahoe Valley Area Plan.

Policy NCR-3.1 – Scenic

Improve the visual quality of the built environment consistent with the general recommendations for site planning found in the TRPA Scenic Quality Improvement Program (SQIP) for Scenic Roadway Unit #1, Unit #35, and Unit #36A.

Policy NCR-3.2 – New Scenic Resources

Designate the views of Tahoe Mountain and Monument Peak and Heavenly Valley as scenic resources.

Policy NCR-3.3 – Viewshed Protection

Adopt design standards and guidelines to preserve, improve, and enhance the scenic quality of the natural environment and take full advantage of scenic views in the Tahoe Valley area through building orientation, building setbacks, and building design.

Policy NCR-3.4 – Open Space

Maintain existing open spaces, stream environment zones, and stormwater drainage basins as view corridors to provide visual relief from the urban environment.

Goal NCR-4: Stream Environment Zones

To protect Stream Environment Zone (SEZ) restoration projects and areas of undisturbed SEZ within the Tahoe Valley area.

Policy NCR -4.1 – Stream Environment Zones

Increase the area of naturally functioning SEZ by preserving existing SEZ and restoring/rehabilitating disturbed SEZ in the Tahoe Valley area, prioritizing the Greenbelt area.

Policy NCR -4.2 – Redirection and Transfer of Development

Direct new development away from SEZs and encourage the removal and transfer of existing development from disturbed SEZs that can be feasibly restored. Encourage property owners to use TRPA's transfer incentives available in Chapter 51 of the TRPA Code. Development rights and existing development that are transferred from SEZs and other sensitive lands are eligible to earn residential bonus units, commercial floor area, and tourist accommodation bonus units, when transferred to Centers.

Policy NCR-4.3 – Stream Environment Zone Mapping and Restoration

Collaborate with TRPA to update the 1987 Bailey Mapped Land Capability Map to more accurately delineate the SEZs within the Tahoe Valley area. Coordinate with public agencies and private property owners to prioritize and target the removal of development and land coverage from the delineated SEZs and from the TRPA designated Stream Restoration Plan Areas.

Goal NCR-5: Water Quality

To protect and enhance Lake Tahoe's unique transparency, color and clarity, and improve the water quality in the south shore's rivers, creeks, marshes, and groundwater.

Policy NCR-5.1 – System

Construct the Tahoe Valley Water Quality Improvement Project to treat stormwater from Tahoe Valley and adjacent residential areas, in order to reduce fine sediment loads to the Upper Truckee River and Lake Tahoe, and restore disturbed SEZs.

Policy NCR-5.2 – Low Impact Development (LID) Techniques

Incorporate site design elements to mimic and enhance natural hydrologic functions such as preserving and protecting environmentally sensitive site features and providing runoff storage, infiltration and groundwater recharge BMPs to reduce the transport of runoff and pollutants from the site at which they are generated.

Policy NCR-5.3 – Best Management Practices

Ensure installation and maintenance of Best Management Practices (BMPs) as required by the City's NPDES Permit and memorandums of understanding with TRPA.

Policy NCR-5.4 – Best Management Practices Retrofit Certification

Coordinate with TRPA to support and/or enforce private property BMP certification with a near-term focus on commercial and multi-family land uses that have drainage directly connected to Lake Tahoe and/or the Upper Truckee River, or are in the same catchment as Environmental Improvement Program (EIP) or public water quality improvement projects.

Policy NCR-5.5 – Environmental Improvement Program (EIP) Coordinate with TRPA to implement the EIP water quality improvement projects in the Tahoe Valley area, with priority on Total Maximum Daily Load (TMDL) pollutant loads reduction opportunities.

Policy NCR-5.6 – Lake Tahoe Total Maximum Daily Load

Collaborate with Lahontan Water Quality Control Board and refine the *Pollutant Load Reduction Strategy* for load reduction targets beyond the year 2016 and update the *Pollutant Load Reduction Plan* as necessary to achieve the Lake Tahoe TMDL load reduction targets. The Tahoe Valley Area Plan hereby incorporates by reference all monitoring, operations, maintenance, and reporting required by the City's NPDES permit, the adopted *Pollutant Load Reduction Plan* and the *Stormwater Management Plan*, which will also be utilized by TRPA in the 4-year Area Plan recertification process pursuant to TRPA Code Sections 13.8.2 and 13.8.5.

Policy NCR-5.7 – Source Water Protection

Prevent contamination of source water in the Tahoe Valley area by requiring projects located in a Source Water Protection Zone, as defined by Chapter 60 of the TRPA Code, to incorporate appropriate source water protection and site-specific best management practices including pretreatment technologies.

Policy NCR-5.8 – Drainage Incentive

Allow projects within the Tahoe Valley area, to provide in-lieu funding for an area-wide municipal stormwater treatment if sufficient treatment capacity exists within an area-wide water quality treatment system, subject to the requirements of TRPA Code Section 13.5.3.B.3. Properties participating in an area-wide system would be considered as meeting their water quality mitigation and BMP requirements of the TRPA Code. Require that full pretreatment of drainage occurs on a private property before it is accepted into a City system.

Policy NCR-5.9 – Off Site Improvements

Ensure that all new development or redevelopment projects, and projects subject to a special use permit install and maintain off-site water quality improvements, including but not limited to curb, gutter, drainage facilities, stabilized road shoulders, at the direction of the City engineer.

Goal NCR-6: Historic Resources

To preserve cultural and historic resources in the Tahoe Valley area.

Policy NCR-6.1 – Resource Protection

Identified historic and cultural resources shall be preserved where feasible. Where it is found that a resource has no economically viable future use, mitigation for the loss of the resource that would preserve public access to its historic or cultural significance shall be required.

Goal NCR-7: Excess Coverage Mitigation

To maximize opportunities to remove and mitigate excess land coverage and facilitate environmental redevelopment in the Tahoe Valley area and to benefit environmental thresholds.

Policy NCR-7.1 – Land Coverage

Onsite land coverage reduction will occur primarily through environmental redevelopment by providing development incentives in centers that promote reduction, relocation and transfer of land coverage. The City will endeavor to reduce overall coverage in order to benefit the objectives of the Tahoe Valley Area Plan.

Policy NCR-7.2 – Land Coverage Management System

- A. To promote revitalization in the Tahoe Valley area and promote opportunities for coverage reduction and SEZ restoration, allow projects within the Town Center Core District to go beyond 70 percent and up to 90 percent of the project area that is located within land capability districts 4-7, inclusive, using 2.5:1 transfer ratio for coverage from non-sensitive lands, and 1:1 transfer ratio for coverage from sensitive lands.
- B. To promote SEZ restoration in the Tahoe Valley Area and the Upper Truckee Hydrologic watershed, all projects transferring or removing coverage from designated sending areas may earn bonus coverage, subject to availability, from the City’s coverage pool. Bonus coverage may be earned pursuant to the transfer ratios in the following table:

Sending Area	Bonus Coverage Earned (per sq. ft.)
TRPA Designated Stream Restoration Plan Area	1
Tahoe Valley Greenbelt and Associated SEZs	1
Other SEZs in the Upper Truckee Watershed Hydrologically Related Area Located within the City Limits	0.5

Policy NCR-7.3 – Landscape Treatment

Encourage onsite landscaping in all private and public redevelopment projects. Landscape features should be integrated with BMPs to treat stormwater runoff and be consistent with fire standards for defensible space.

Policy NCR-74 – Transfer of Coverage

Priority should be given to transferring hard and soft coverage from SEZs and other sensitive land to high capability lands within the Town Center Core District.

Policy NCR-7.5 – Stormwater Enhancement

Collaborate with the California Tahoe Conservancy in identifying priority parcels in sensitive lands for public acquisition and coverage removal to facilitate construction of the Tahoe Valley Water Quality Improvement Project.

Goal NCR-8: Air Quality

To achieve air quality improvement and emission reductions in the Tahoe Valley Area Plan.

Policy NCR-8.1 – Construction Emissions

The City shall incorporate measures to reduce construction-generated emissions to the extent feasible on a project-specific basis. Such measures may include, but are not limited to, the following:

- Implement measures recommended by the El Dorado County Air Quality Management District.
- Prohibit open burning of debris from site clearing unless involved with a fuels reduction project.
- Utilize low emission construction equipment and/or fuels and use existing power sources (e.g., power poles), wherever feasible.
- Restrict idling time for construction equipment and vehicles.
- Apply water to control dust as needed to prevent dust impacts.

10.2 Implementation Strategies

This subsection outlines Natural and Cultural resources improvements that implement the vision, guiding principles and the goals and policies of the Tahoe Valley Area Plan.

SCENIC RESOURCES

As discussed in the Section 3, Existing Conditions, TRPA Threshold Evaluations noted that the majority of roadway units with improved threshold scores fall partially or wholly within urbanized areas. Removal of degraded structures, improvement in architectural quality of new and remodeled structures, increased landscaping, creation of open space and landscape drainage basins, installing curb, gutters and pedestrian sidewalks and consolidating highway curb cuts, and improved signage have all contributed to a transformation in many of these units. This improvement affects both travel route and scenic quality ratings.

Within the Tahoe Valley plan area, continued improvements in the built environment are anticipated through the implementation of the recommendations of the Scenic Quality Improvement Program,

implementation of the design standards of the Area Plan that are intended facilitate the upgrade of the overall architectural quality, the preservation of open space and viewsheds, and installation of curbs and gutters along US 50 and SR 89. The following implementation strategies are also intended to promote scenic threshold attainment:

- Zone SEZ restoration sites and stormwater drainage basins as Open Space in the Tahoe Valley Area Plan.
- Designate the following as scenic resources (See Figure 7-1):
 - The mountain and ridgeline views of Tahoe Mountain as viewed from the Y intersection.
 - The mountain and ridgeline views of Monument Peak and Heavenly Ski Resort from the Y intersection.
- Incorporate design standards that enhance the scenic quality of the natural and built environment in Tahoe Valley for the following elements:
 - Architectural Style
 - Building Materials
 - Building Colors
 - Building Height
 - Access Drives
 - Parking
 - Lighting
 - Landscaping
 - Signs
 - Utility Lines
- Install curb and gutter and sidewalks as part of the Caltrans US 50 and SR 89 Water Quality Improvement Projects.
- Implement the City of South Lake Tahoe’s Business Façade Improvement Program to provide low-interest loans to low income businesses and Business Improvement Districts to aesthetically improve building facades.
- Designate new scenic resources within the Tahoe Valley Area Plan, as shown in Figure 11.

STREAM ENVIRONMENT ZONES

The following programs and key strategies are intended to promote SEZ restoration threshold attainment in the Tahoe Valley area:

- Restore disturbed SEZ as part of the construction of the Tahoe Valley Greenbelt and Tahoe Valley Water Quality Improvement Project.

- Coordinate with the California Tahoe Conservancy to prioritize and target the removal of development and land coverage in the delineated SEZs and from the TRPA designated Stream Restoration Plan Areas.
- Amend TRPA's 1987 Bailey Mapped Land Capability Map to adopt an updated map that more accurately delineates the boundaries of the SEZs that are located in the Tahoe Valley area.

There has been extensive disturbance of SEZs in the planning area. The opportunities to implement area-wide BMPs, improve drainage systems, and increase SEZ restoration to benefit stream environment zone function exist in the planning area, but are dependent upon verifying and delineating accurate SEZ boundaries. The City hired Cardno/Entrix and Terra Sciences, experts in the field of soil science and hydrology, to review the existing mapped SEZs and verify the land capability districts for the Tahoe Valley Area Plan. The consultants analyzed previous land capability verifications, site assessments and land capability challenges that have occurred in the planning area, conducted a visual inspection of SEZs (reviewing vegetation, landforms, topography and aerial photography, and examined soil samples to closely examine near-surface soil conditions). The experts concluded that the SEZ boundaries adopted in the 1987 Land Capability Map are not accurate to the Area Plan scale. These efforts have resulted in a new SEZ and land capability district map, which is included as Figure 14. It should also be noted that even with an updated land capability map, property owners are still required to obtain parcel level site assessments and/or land capability verifications from TRPA prior to project submittal.

WATER QUALITY

The Tahoe Valley Area Plan responds to the needed improvements in water quality by continuing to require installation of on-site BMPs, construction and maintenance of water quality improvement projects within the Tahoe valley area and implementation of the City's TMDL Pollutant Load Reduction Plan. The following programs and key strategies are intended to promote water quality threshold attainment.

BMPs

- Focus BMP installation and compliance on commercial and multi-family projects within the Tahoe Valley area which have drainage directly connected to Lake Tahoe and/or the Upper Truckee River or are in the same catchment as EIP or public projects.
- The City shall require BMPs to be installed and maintained in all projects reviewed under the City's MOU to capture a 20-year, one-hour storm or alternative BMPs if infiltration is not possible. Properties participating in an area-wide system would be considered as meeting their water quality mitigation and BMP requirements of the TRPA Code.
- Coordinate with TRPA to support private property BMP certification with a near-term focus on approximately 290 acres of commercial land uses that directly discharge stormwater to Lake Tahoe.

EIP Projects

Implement the following Water Quality Improvement Projects (WQIPs):

- US 50 Water Quality Improvement Project: Caltrans will treat storm runoff from US 50 from the Y intersection to the airport and from the Y intersection to Trout Creek. The section from the Y intersection to the airport is expected to be completed in October 2014 and the segment from the Y intersection is due for completion in 2019.
- SR 89 Water Quality Improvement Project: Caltrans will treat storm runoff from SR 89 from the Y intersection Cascade Road. The project will begin in 2014 and is due for completion in October 2015.
- The Tahoe Water Quality Improvement Project: The project focuses on two key elements: (1) the construction of a comprehensive regional storm water treatment; and (2) construction of a recreation and open space community amenity. The Tahoe Valley Stormwater Improvement Project includes much of the densely developed commercial areas near U.S. Highway 50, both north and south of the “Y”. The area is a priority for treatment as storm water runoff discharges directly to the Upper Truckee River, which drains to Lake Tahoe. The project includes design of water quality improvements to reduce pollutant loads discharged to the Upper Truckee River.

Tahoe TMDL

Implement the recommendations identified in the City of South Lake Tahoe Pollutant Load Reduction Plan (PLRP) for achieving the 10 percent load reduction target for the first five-years.

The City’s selected approach to meet load reduction targets combines the registration of Water Quality Improvement Projects (WQIP) completed from 2004-2016 with the implementation of a pilot program to improve road operations for water quality. Specific actions relevant to this Area Plan include the following:

- Register Completed WQIPs (2004-2012): The City has completed seven WQIPs. The Urban Planning Catchments (UPCs) that encompasses the seven completed project areas will be registered with the Lake Clarity Crediting Program. Of the seven WQIP projects that will be registered, the Glorene and 8th Street WQIP is located within the Tahoe Valley planning area and treats runoff from within the plan area and from adjacent neighborhoods.

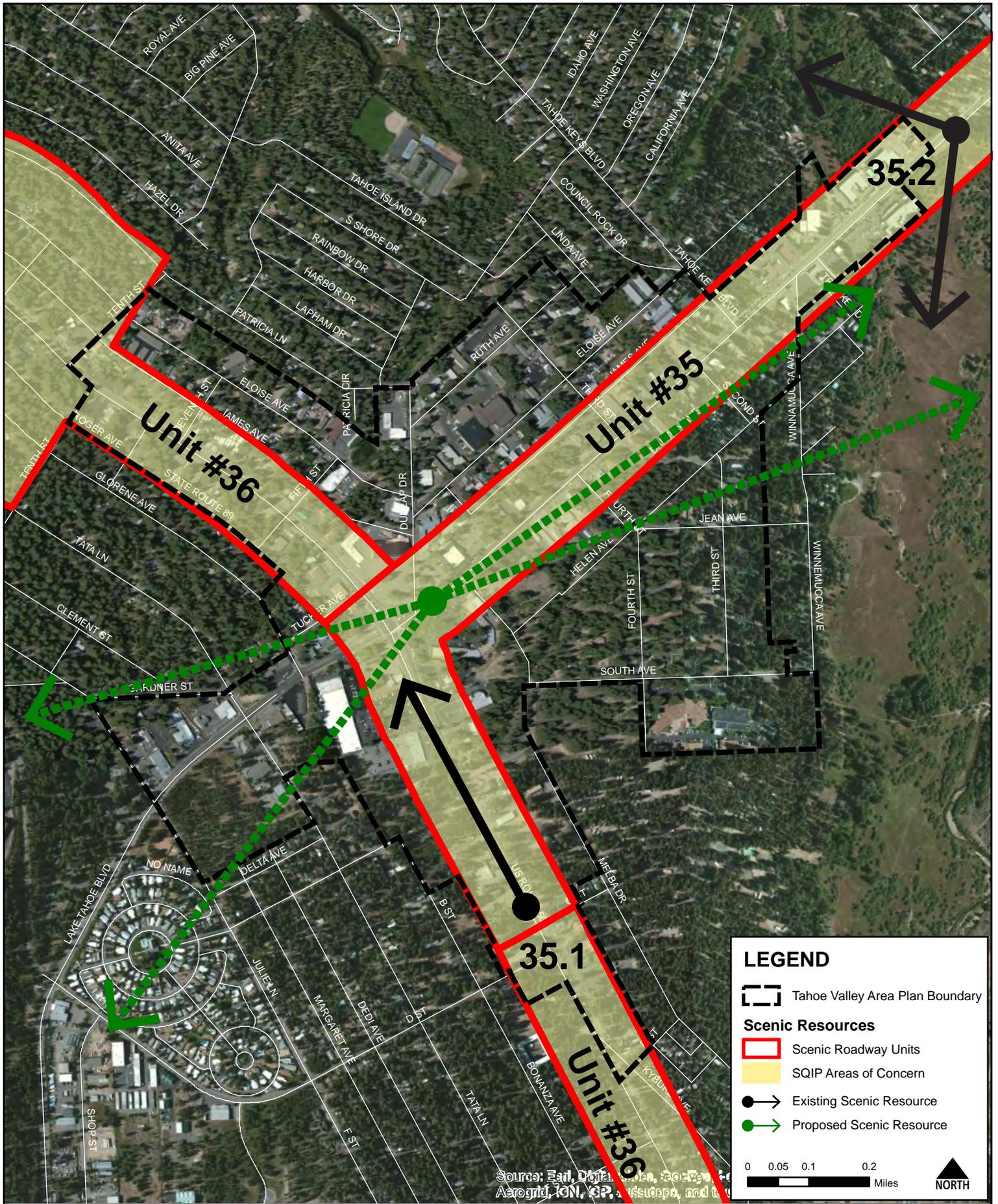
Using the Lake Clarity Credits as the defining performance metric, the Pollutant Load Reduction Model (PLRM) analysis estimates that the City will obtain the necessary credits to achieve the 10 percent reduction by 2016.

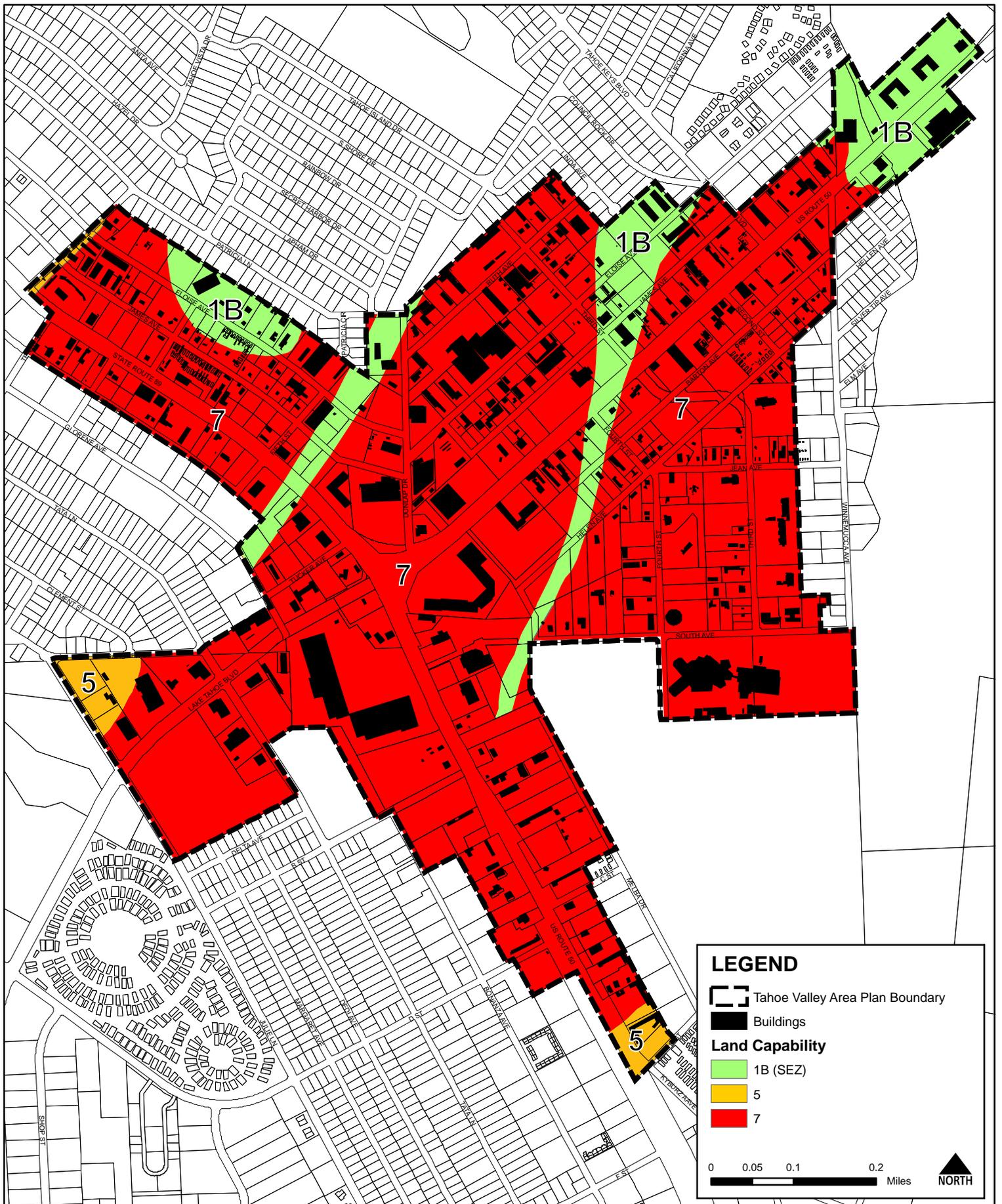
In addition to the load reduction targets, the Municipal Separate Storm Sewer System (MS4) permit issued by the Lahontan Water Quality Control Board to the City of South Lake Tahoe identified a number of milestones for load reduction planning efforts, including: reporting of waste discharge and updating the PLRP by June 9, 2016.

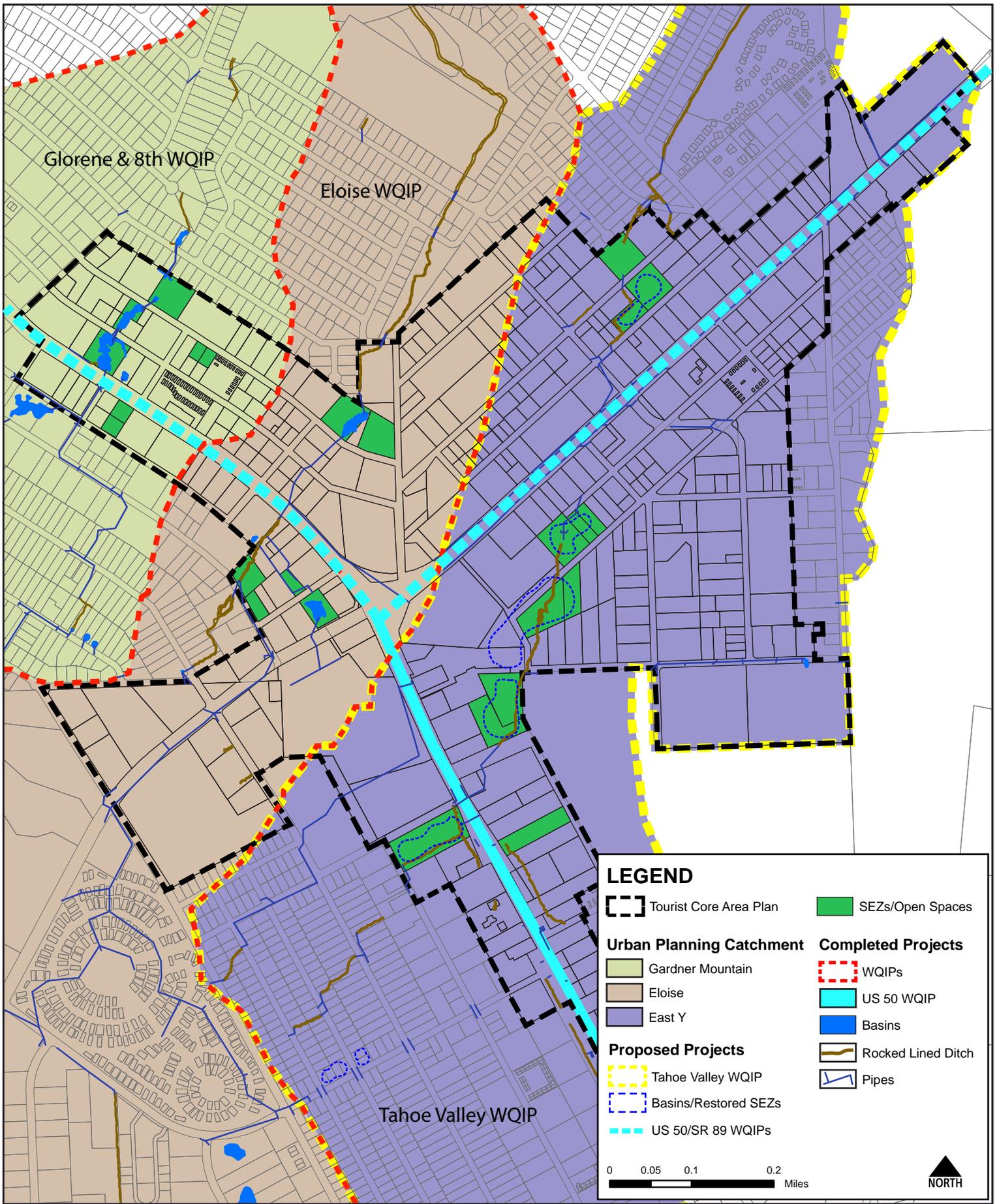
The City will continue to track abrasive applications and materials recovered by sweeping and vactoring, which helps identify areas with high pollutant loading. The City will continue to inspect facilities during large runoff events, to determine whether constructed source control, conveyance, and treatment measures are functioning as designed. The City will also complete annual stormwater system, construction and commercial, industrial, and municipal site inspections required in the MS4 permit.

These activities form the foundation of the City's internal approach to assess its stormwater management activities and provide data to support assessments and reporting of load reduction progress during the implementation of the PLRP. Moreover, it provides valuable data for updating of the PRLP for year 2016 and beyond.

Figure 15 depicts proposed EIP and TMDL water quality improvement projects within the plan area and its vicinity to be implemented to improve the water quality, soils and SEZ thresholds.







SECTION 11

Recreation and Open Space



The Recreation and Open Space Section is a supplement to the Recreation and Open Space Element of the City of South Lake Tahoe General Plan and the TRPA Regional Plan Recreation Element. Consistent with these plans, this section provides the policy context for the Tahoe Valley Area Plan to improve recreational opportunities.

Tahoe Valley serves as a hub for numerous outdoor recreation activities. A variety of recreational opportunities, such as boating, campgrounds, beaches, and mountain bike trails are easily accessible from the plan area. Public beaches, campgrounds, trail heads, interpretive centers, and boat launching facilities are all located in close proximity to Tahoe Valley and are accessible by bike paths and routes that run through the plan area and by transit from the South Y Transit Center. In addition, Tahoe Valley has numerous established businesses that cater to the recreation user by providing recreation based commercial services such as the sale and rental of biking, kayaking, and paddle board equipment. To further expand and enhance the recreation opportunities in the plan area and its surroundings, the goals, policies, and implementation strategies of this section promote the preservation and creation of open space, construction of bike and trail connections, increased transit service, and establishment of eco-tourism and recreation based businesses within the plan area.

11.1 Goals and Policies

This subsection outlines recreation goals and policies for the Tahoe Valley Area Plan. These goals and policies implement the vision and guiding principles discussed earlier.

Goal REC-1: Recreation System

To create a recreation system for the enjoyment of both local residents and tourists.

Policy REC-1.1 – Hierarchy and Interconnection

Develop an interconnected system of open spaces, community parks, plazas, bike and pedestrian trails, and other types of public and private spaces as part of new development and redevelopment of existing sites.

Policy REC-1.2 – Coordination with Commercial Uses

Require public trails and plazas to be incorporated into the development and redevelopment of commercial, industrial, public, and multi-family projects.

Policy REC-1.3 – Private Improvements

Encourage the private sector to make recreational improvements on their land. Improvements may include establishing community event areas, providing paths and walkways that connect to adjacent sidewalks and bike paths, establishing appropriate space for the outdoor display of for

sale or rent recreation equipment.

Policy REC-1.4 – Access

Enhance the existing trail and bike system that connects the Tahoe Valley area to recreation opportunities in the south shore.

Policy REC-1.5 – Eco-Tourism Incentives

Businesses and facilities that promote the South Shore as an eco-tourism recreation destination qualify for the following incentives:

- Projects would receive priority plan check, over all other project applications, by all City Departments.
- Project would receive recognition at a televised City Council meeting and on the City Website.

Goal REC-2: Open Space

To preserve, protect, and maintain open spaces in the Tahoe Valley plan area.

Policy REC-2.1 – Preservation

Preserve and maintain open spaces within the Tahoe Valley plan area that provide a visual break from the urban environment as well as views of surrounding mountains, forests, and stream environment zones. Depending upon their ecological sensitivity, open spaces may incorporate paths, boardwalks, pedestrian and bicycle trails, and benches.

Policy REC-2.2 – Public Access

Ensure that public access does not jeopardize restoration activities and water quality and stormwater treatment projects on publicly-owned open space lands.

Policy REC-2.3 – Multi-Use

Encourage the multi-use of open spaces in the Tahoe Valley area for storm water treatment, pedestrian and bicycle connectivity, passive recreation uses and as a visual break from the developed environment.

Policy REC-2.4 – Tahoe Valley Greenbelt

Construct the Tahoe Valley Greenbelt Project as multi-use area incorporating stormwater treatment facilities, pathways, and open space. Allow for the accommodation of active and passive recreation uses that include but are not limited to installing bike/pedestrian paths, pedestrian amenities, and interpretive signage.

Policy REC-2.5– Adjacent Uses

Encourage commercial uses located adjacent to the Tahoe Valley Greenbelt to provide direct access to the greenbelt and use it as a visual amenity.

Policy REC-2.6– Coordination

Coordinate with the California Tahoe Conservancy to ensure that implementation of the Tahoe Valley Greenbelt is consistent with recreation and restoration goals and objectives of the California Tahoe Conservancy and the State of California

Goal REC-3: Recreation Opportunities

To connect Tahoe Valley to a network of parks, open spaces, and recreation sites that serve the needs of South Lake Tahoe residents and tourists.

Policy REC-3.1 – Recreation Hub

Promote the Town Center Core District as a recreation hub by allowing for a mix of uses that promote outdoor recreation concessionaires, recreation based commercial services, and temporary activities. Connect the recreation hub to recreation opportunities by upgrading existing bike and pedestrian trails, constructing new connections, and increasing transit service. Improve access to recreation opportunities by installing way-finding and interpretive signage within the Town Center Core District and improve access along bike trails.,

Policy REC-3.3 – Neighborhood/Pocket Parks

Develop neighborhood/pocket parks within the Neighborhood Professional District and surrounding residential areas.

Goal REC-4: Plazas

To provide area within commercial centers for people to meet, socialize, relax, and interact.

Policy REC-4.1 – Plaza Location

Require all large development within the Town Center Core District to provide informal public gathering areas and places for community activities.

Policy REC-4.2 – Plaza Amenities

Require all plazas to include seating, tables, and other amenities attractive to shoppers and pedestrians. Larger plazas should include trees, native landscaping, hardscape, year-round restrooms, formal eating areas, places for community activities, and possibly areas for spectator recreational activities such as climbing walls.

Goal REC-5: Temporary Events

To provide areas for temporary events in the Town Center Core District.

Policy REC-5.1 – Temporary Event Locations

Encourage temporary events and activities within public and private plazas in the Town Center Core District to increase pedestrian and economic activity in commercial areas.

11.2 Implementation Strategies

This subsection outlines Recreation improvements that implement the vision, guiding principles and the goals and policies of the Tahoe Valley Area Plan.

In addition to the implementation strategies proposed in the transportation section to enhance recreation opportunities, the following recreation projects or programs are planned:

- The Tahoe Valley Greenbelt: The project focuses on two key elements: (1) the construction of a comprehensive regional storm water treatment system (see Tahoe Valley Water Quality Improvement Project description in Section 10, page 74); (2) Improvement of the bicycle and

pedestrian transportation system; and (3) construction of a recreation and open space community amenity southeast of the “Y” intersection. The project will include SEZ restoration, installation of pedestrian-bicycle pathways, pedestrian amenities and interpretive signage along with storm water improvements. The Greenbelt will serve as a visual amenity for adjacent residential and commercial uses and provide a bicycle and pedestrian link between the residential neighborhoods and the Tahoe Valley commercial core. There are existing bicycle paths and stormwater facilities in the area, which would be upgraded and enhanced as part of this project. Conceptual illustrations of the Greenbelt are depicted in Figures 16 and Figure 17.

LEGEND

Tahoe Valley Area Plan Boundary

Existing Facilities

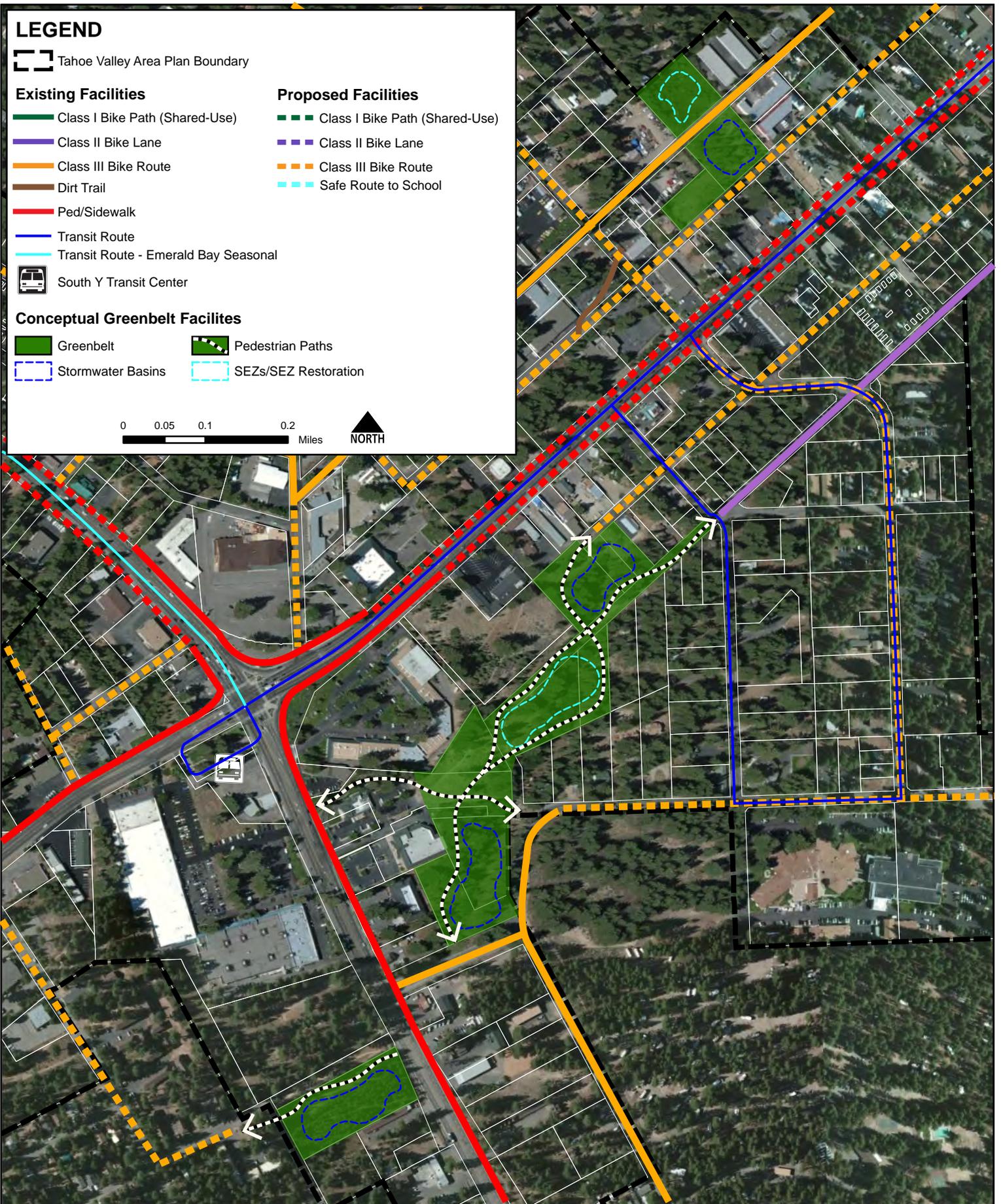
- Class I Bike Path (Shared-Use)
- Class II Bike Lane
- Class III Bike Route
- Dirt Trail
- Ped/Sidewalk
- Transit Route
- Transit Route - Emerald Bay Seasonal
- South Y Transit Center

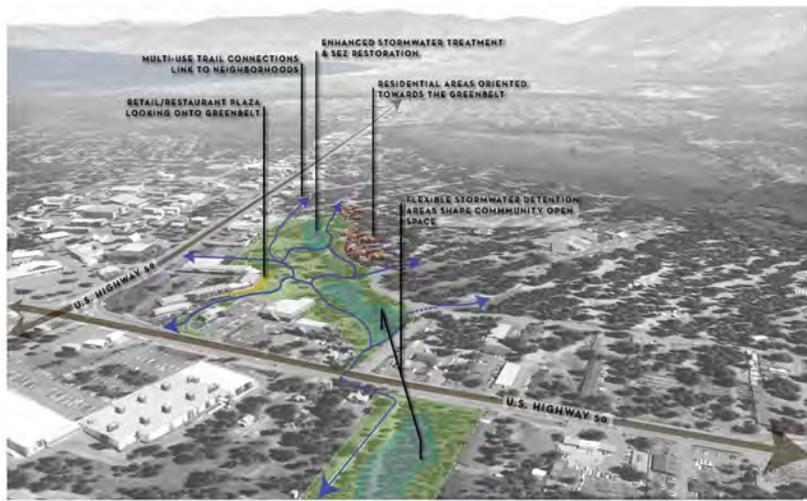
Proposed Facilities

- Class I Bike Path (Shared-Use)
- Class II Bike Lane
- Class III Bike Route
- Safe Route to School

Conceptual Greenbelt Facilities

- Greenbelt
- Pedestrian Paths
- Stormwater Basins
- SEZs/SEZ Restoration





SECTION 12

Safety and Noise



The Safety and Noise Section is a supplement to the Health and Safety Element of the City of South Lake Tahoe General Plan, and the Natural Hazards Element of the TRPA Regional Plan. Consistent with these plans, this section provides the policy context for the Tahoe Valley Area Plan for future health and safety. Safety hazards are naturally induced, such as seismic, geologic, flooding, snow, avalanche, and wildland fire hazards. Other hazards are human-made including aircraft crash hazards and hazardous materials. In addition, the planning process should account for other issues related to community health and safety, including noise exposure. Protection from the risks of natural and human-made hazards is essential in establishing a sense of well-being for residents and visitors.

12.1 Goals and Policies

This subsection outlines safety and noise goals and policies for the Tahoe Valley Area Plan. These goals and policies implement the vision and guiding principles discussed earlier.

Goal HNS-1: Safety

To protect the community from injury and damage resulting from natural catastrophes and hazardous conditions.

Policy HNS-1.1 – Overflight Zones

Require all new projects located within the overflight safety area to meet the standards and requirements of the South Lake Tahoe Comprehensive Land Use Plan (CLUP) in effect at the time the project is reviewed.

Policy HNS-1.2 – Floodplains

Support the restoration of floodplains through the use of incentives to relocate existing development to higher capability lands.

Policy HNS-1.3 – Fire Protection

Require any new structure or any addition that intends to construct to alter or repair, to implement fire prevention techniques consistent with the current California Building and Fire Codes in order to obtain a building permit

Policy HNS-1.4 – Defensible Space and Fire-Safe Landscaping

Require property owners to maintain defensible space through annual landscape maintenance, including the removal of dead/dying material and pruning/thinning of flammable vegetation. Ensure that landscape plans in new and rehabilitated structures conform to defensible space requirements of Section 4291 of the California Public Resources Code.

Policy HNS-1.6 – Water Lines

Require all public water providers to maintain adequate water supply systems and flows to meet fire suppression needs throughout the city.

Policy HNS-1.7 – Minimum Fire Flow Requirements

Require that all new construction meet the minimum fire flow requirements as set forth in the current California Building and Fire Codes.

Policy HNS-1.8 – Healthcare Campus District

Collaborate with Barton Healthcare System to ensure adequate access is provided for emergency vehicles in any new or reconstruction of facilities in the Healthcare District.

Policy HNS-1.9 – Tahoe Valley Greenbelt

Ensure adequate access for paramedic and fire vehicles to the Tahoe Valley Greenway. Ensure that the greenway can be access from adjacent streets and that bike and pedestrian paths can accommodate emergency vehicles.

Goal HNS-2: Noise

To protect residents from the harmful and annoying effects of exposure to excessive noise.

Policy HNS-2.2 – Maximum Community Noise Equivalent Level

Maintain a maximum community noise equivalent level (CNEL) of 65 CNEL for the Tahoe Valley Area, except for the Neighborhood Professional and Healthcare Campus Districts, where the standard is 55 CNEL. Require any new project that is determined that have the potential to exceed the noise standards submit a noise analysis as part of the environmental review process. Noise measurements conducted for noise analyses must follow TRPA protocols for the CNEL measurements. These projects must incorporate design features to ensure compliance with noise standards.

Policy HNS-2.1 – Transportation Noise

The City shall require the mitigation of new transportation noise sources to the levels shown in Table HS-2 of the City General Plan at all outdoor activity areas and interior spaces of existing noise-sensitive land uses.

Policy HNS-2.1 – Project Review

The City shall require an acoustical analysis as part of the environmental review process when noise-sensitive land uses are proposed in areas exposed to existing or anticipated exterior noise levels exceeding the levels shown in Table HS-1 and HS-2 of the City General Plan, so noise mitigation may be included in the project design. All acoustical analysis shall:

- Be the financial responsibility of the applicant
- Be prepared by a qualified person experienced in the fields of environmental noise assessment and architectural acoustics.
- Include representative noise level measurements with sufficient sampling periods and locations to adequately describe local conditions and the predominant noise sources; and
- Estimate existing and projected cumulative (20-year) noise levels in terms of Ldn or CNEL and/or the standards shown in Table HS-1, and compare those levels to the policies in this

section;

- Recommend appropriate mitigation to achieve compliance with the adopted policies and standards of this section, giving preference to proper site planning and design over mitigation measures which require the construction of noise barriers or structural modifications to building which contain noise-sensitive land uses;
- Estimate noise exposure after the prescribed mitigation measure(s) has been implemented; and
- Describe a post-project assessment program that could be used to evaluate the effectiveness of the proposed mitigation measure(s).

Policy HNS-2.2 – Construction Noise

The City shall require an analysis of potential vibration impact be conducted for all construction activities that include impact equipment and activities such as pile driving, soil compaction, or vibratory hammers that occur within 200 feet of existing structures. The analysis will address the potential for adverse vibration levels based on the criteria contained in Table 4.6-12 of the City General Plan Draft EIR. The City will ensure that construction operations are designed to avoid or mitigate for vibrations above 0.02 in/sec. (0.5 mm/sec.)

Policy HNS-2.2 – Land Use Compatibility

The City shall incorporate measures to ensure noise/land use compatibility on a project-specific basis. Such measures may include, but are not limited to , the following:

- Construction of barriers, berms, and/or acoustical shielding;
- Establishment of setback requirements for new development in specific areas exposed to roadway noise;
- Noise-reducing acoustical treatment of existing and proposed buildings; and
- Use of building layout and project features for noise-screening purposes.

SECTION 13

Implementation

This section sets how the goals and policies of the Tahoe Valley Area Plan are to be implemented. The section describes environmental and capital improvement projects, mitigation fee programs, and incentive programs to implement the policies of this Area Plan to create a sustainable, walkable, connected community through environmental redevelopment while achieving threshold gains.

13.1 Capital and EIP Programs

Transportation and Circulation

To improve traffic, circulation, pedestrian mobility and air quality in the area, the following improvements are planned.

Improvements	Description	Responsible Party
US 50 and SR 89 Improvements	<u>US 50 Airport to "Y" Junction Water Quality Improvement Project:</u> As part of this water quality improvement project, Caltrans will install a 3- to 4-foot shoulder for a Class II bike lane, and will provide curb, gutter and sidewalks. The project is currently under construction and the bike lanes, curb, gutter and sidewalks have been installed. Project completion is expected in the October 2014.	CALTRANS
	<u>US 50 "Y" to Trout Creek Water Quality Improvement Project:</u> As part of this water quality improvement project, Caltrans will install 6-foot shoulders for Class II bike lanes, and replace traffic signals, curbs, gutters and sidewalks. The project is currently in the design phase and construction is expected to begin late 2016 and be completed by 2019.	CALTRANS
	<u>SR 89 "Y" to Cascade Road Water Quality Improvement Project:</u> As part of this water quality improvement project, Caltrans will install a 4-foot shoulder for a Class II bike lane. The project has received a conditional construction permit from TRPA and construction is expected to begin the Fall of 2014 and be completed by the October 2015.	CALTRANS

Improvements	Description	Responsible Party
	<p><u>RTP #74: US 50 Signal Synchronization and Adaptive Signals Project.</u> Upgrade signal timing equipment at signalized intersections along US Highway 50 to improve traffic flow.</p>	CALTRANS
	<p><u>RTP #81: Signalization Upgrade.</u> Upgrade signal timing equipment at City of South Lake Tahoe signalized intersections to improve traffic flow.</p>	CALTRANS
	<p><u>Tahoe Keys Intersection Improvements:</u> Install a continuous right turn lane on US 50 at the Tahoe Keys Intersection.</p>	CALTRANS CSLT
	<p><u>South “Y” Intersection Bike Lane Stripping.</u> Coordinate with Caltrans to consider upgrading the current bike lane stripping at the South “Y” intersection to install a combined bike lane/turn lane, intersection crossing markings, and colored pavement to enhance public safety and clearly demark auto and bicycling travel lanes</p>	CALTRANS CSLT
Bike Paths, Lanes and Routes Improvements	<p><u>The Tahoe Valley Greenbelt:</u> The project focuses on two key elements: (1) the construction of a comprehensive regional storm water treatment system (see Tahoe Valley Erosion Control Project description); and (2) construction of a recreation and open space community amenity. This project will create a distinct and unique recreation and open space amenity southeast of the “Y” intersection. The project will include SEZ restoration, installation of pedestrian-bicycle pathways, pedestrian amenities and interpretive signage along with storm water improvements. The Greenbelt will serve as a visual amenity for adjacent residential and commercial uses and provide a bicycle and pedestrian link residential neighborhoods to the Tahoe Valley commercial core. There are existing bicycle paths and stormwater facilities in the area, which would be upgraded and enhanced as part of this project.</p>	CSLT CTC
	<p><u>South Tahoe Greenway:</u> The proposed South Tahoe Greenway Shared Use Trail will create the backbone of the bicycle and pedestrian network in South Lake Tahoe and will take a major step forward in furthering sustainability goals of the south shore communities. The Greenway will connect residents and visitors to community and recreation destinations from Myers to the Stateline, providing a high quality alternative to private automobile use. The highest priority 1.8-mile segment will link urban neighborhoods in the core of South Lake Tahoe to shopping, social services, schools, and recreation destinations. Continuing the trail</p>	CSLT

Improvements	Description	Responsible Party
	<p>system northward to the state line provides important connections to the largest concentration of visitor services in the region, including shopping, lodging and transit. Completion of this link also connects the recreation resources at Van Sickle Bi-State Park to the network. A southern trail extension to Meyers, not part of the current project, would complete the transportation link.</p>	
	<p><u>Bike Path Rehabilitation</u>: This project focuses on rehabilitating and laying new asphalt on an existing Class I bike path that runs from Trout Creek to the Y intersection. One segment is located adjacent to the Tahoe Valley Area Plan behind Motel 6 and another segment is located in the plan area boundary and runs from Helen Avenue to the Y intersection. Project completion is expected in October 2014.</p>	CSLT
	<p><u>Bicycle Transportation Signage System</u>: The City is coordinating with the Lake Tahoe Bicycle Coalition to develop and implement a consistent, functional bicycle transportation signage system that serves the commuting needs of employees, students, business owners, shoppers and tourists in the South Shore. The intent of the program to implement a consistent way-finding signage system that can direct cyclist and pedestrian traffic to services, places of employment, and recreation opportunities in a safe and efficient manner.</p>	LTBC CSLT
	<p>Coordinate with Caltrans to evaluate alternative crossings that create safe passage across US 50 and SR 89 for pedestrians and cyclist.</p>	CSLT CALTRANS
	<p>Sign Class III bike lanes as depicted in Figure 12 of the Tahoe Valley Area Plan.</p>	CSLT
Pedestrian Improvements	<p>Install sidewalks along US 50 and SR 89 as part of the US 50 and SR 89 Water Quality Improvement Project.</p>	CALTRANS

TRANSIT IMPROVEMENTS

To reduce vehicle trip ends and level of service targets; transit services should be improved.

Improvements	Description	Responsible Party
Transit Service	South Lake Tahoe Bus Shelters. The Tahoe Transportation District is continuing a program of constructing new shelters in the South Shore. The structures will enhance rider comfort and safety while adding visibility to encourage transit use.	TTD
	Continue bus service from the Tahoe Valley Transit Center to Emerald Bay during the summer season.	TTD

STREAM ENVIRONMENT ZONE RESTORATION PROGRAM

The purpose is to restore as much SEZ as possible where feasible to help meet TRPA threshold targets.

Improvements	Description	Responsible Party
SEZ Restoration	<u>The Tahoe Valley Greenbelt</u> : Restore disturbed SEZ as part of the construction of the Tahoe Valley Greenbelt and Tahoe Valley Water Quality Improvement Project.	CSLT CTC
	<u>Tahoe Valley Land Capability Map</u> : Adopt the updated Land Capability Map and coordinate with the California Tahoe Conservancy to prioritize the removal of development and coverage from delineated SEZs.	CSLT CTC

WATER QUALITY IMPROVEMENTS

The purpose is to implement water quality improvement projects and programs to help achieve TRPA's water quality threshold and the California Regional Water Quality Control Board-Lahontan Region TMDL pollutant load reduction targets.

Improvements	Description	Responsible Party
US 50 and SR89 Water Quality Improvement	<u>US 50 Airport to "Y" Junction Water Quality Improvement Project</u> : The project will collect and treat runoff along US 50. The project will provide a 3- to 4-foot shoulder for a Class II bike lane, and	CALTRANS

Improvements	Description	Responsible Party
Project	will provide curb, gutter and sidewalk. The project is currently under construction and is expected to be completed in the Fall of 2014.	
	<u>SR 89 “Y” to Cascade Road Water Quality Improvement Project</u> : The project will collect and treat runoff along SR 89 from the “Y” junction to Cascade Road. The project will provide curb and gutter and sidewalks, and a 4-foot shoulder for a Class II bike lane. The project has received a conditional construction permit from TRPA and construction is expected to begin the Fall of 2014 and be completed by the Winter of 2015.	
	<u>US 50 “Y” to Trout Creek Water Quality Improvement Project</u> : The project will collect and treat stormwater runoff from the “Y” junction to Trout Creek. It will include a 6-foot shoulders for Class II bike lanes, and replace traffic signals, curbs, gutters and sidewalks. The project is currently in the design phase and construction is expected to begin late 2016 and be completed by Winter of 2019.	
Tahoe Valley Water Quality Improvement Project	<u>The Tahoe Valley Water Quality Improvement Project</u> : includes much of the densely developed commercial areas near U.S. Highway 50, both north and south of the “Y”. The area is a priority for treatment as storm water runoff discharges directly to the Upper Truckee River, which drains to Lake Tahoe. The project includes design of water quality improvements to reduce pollutant loads discharged to the Upper Truckee River.	CSLT
Register Completed WQIPs (2004-2012):	The City has completed seven WQIPs. The Urban Planning Catchments (UPCs) that encompasses the seven completed project areas will be registered with the Lake Clarity Crediting Program. Of the seven WQIP projects completed by the City that will be registered, one is located within Tahoe Valley and treats runoff from within the planning area and from adjacent residential neighborhoods. The WQIP is Glorene and 8 th Street.	CSLT
Stormwater System	City will complete annual stormwater system, construction and commercial, industrial, and municipal site inspections required in the NPDES MS4 permit.	CSLT

Improvements	Description	Responsible Party
BMP Compliance	Focus BMP installation and compliance on commercial and multi-family projects that are adjacent, have connected impervious surfaces, or the same catchment as EIP or public projects.	TRPA
	Ensure BMPs are incorporated in all projects reviewed under the City's MOU to capture a 20-year, one-hour storm or alternative BMPs if infiltration is not possible.	CSLT
	Coordinate with TRPA to support and/or enforce private property BMP certification with a near-term focus on approximately 290 acres of commercial land uses that directly discharge stormwater to Lake Tahoe	CSLT TRPA
Road Operations	The City will continue to track abrasive applications and materials recovered by sweeping and vactoring in order to identify areas with high pollutant loads.	CSLT

SCENIC IMPROVEMENTS

To continue to restore the scenic quality of US 50 and Pioneer Trail scenic roadway units and improve the overall community character within the area plan.

Improvements	Description	Responsible Party
US 50 and SR 89	Install curb, gutter, sidewalks, street lights and landscaping to provide a "clean edge" along the roadway.	CALTRANS CSLT
Development and Design Standards	Implement the development and design standards and guidelines during project review to enhance the built environment, promote pedestrian mobility and provide for scenic viewshed protection.	CSLT
Land Use	Designate SEZ restoration sites and stormwater drainage basins as Open Space in the Tahoe Valley area.	CSLT
Scenic Quality Resources	Designate the following as scenic resources: <ul style="list-style-type: none"> ▪ The mountain and ridgeline viewshed of Tahoe Mountain as viewed from the Y intersection. ▪ The mountain and ridgeline viewshed of Monument Peak and Heavenly Ski Resort from the South "Y" intersection. 	CSLT TRPA
Business Façade	Implement a program to provide low-interest loans to	CSLT

Improvements	Description	Responsible Party
Improvement Program	local businesses and Business Improvement Districts to install aesthetically improved building facades.	
SQIP	Implement the SQIP recommendations in non-attainment scenic roadway units during project review.	CSLT

RECREATION IMPROVEMENTS

To improve access to public recreation opportunities.

Improvements	Description	Responsible Party
Way-Finding Signage Program	Expand the City's Way-Finding program in the Town Center Core District to direct pedestrian and bicycle traffic to recreation opportunities along the southwest shore of Lake Tahoe.	CSLT CALTRANS CTC Private
Parks	Construct the Tahoe Valley Greenbelt and provide pedestrian and bicycle connectivity, passive recreation uses and provide a visual amenity for adjacent land uses.	CSLT CTC
Commercial Floor Area	Amend the CFA allocations procedures to add additional criteria for earning CFA from the City unallocated pool. The City should add criteria that allow projects that promote the South Shore as an eco-tourism recreation destination to access the City's CFA pool	CSLT

13.2 Economic and Sustainability Programs

To promote economic activity and increase pedestrian activity in the Tahoe Valley area.

Improvements	Description	Responsible Party
Special Event Area Designation	The City shall pursue obtaining a TRPA special event permit designation for the Town Center Core District. Temporary activities may occur in the special event area without further TRPA review upon approval of a special event area. All other temporary events outside the identified special event area will require a temporary activity permit.	CSLT
Commercial Floor Area	Amend the CFA allocations procedures to add additional criteria for earning CFA from the City unallocated pool.	CSLT

Improvements	Description	Responsible Party
	The City should add criteria that allow projects that voluntarily achieve a LEED or Energy Star green building certification to access the City's CFA pool.	
Land Coverage	Implement the Land Coverage Management System to leverage the City's banked coverage and SEZ restoration credits to facilitate redevelopment, land coverage removal, and SEZ restoration in the Tahoe Valley area.	CSLT

13.3 Incentive Programs

Specific incentives are available to support implementation of the plan's vision and support attainment of TRPA's Thresholds. Some of these incentives are found in the plan itself, others are found in the TRPA Code of Ordinances, and additional incentives are provided through City programs.

Alternate Water Quality Fee Program

An Area Plan may propose to establish area-wide water quality treatments and funding mechanisms in lieu of certain site-specific BMPs, subject to the requirements outlined in TRPA Code section 13.5.3.B.3. All properties within the Tahoe Valley area plan that participate in an area-wide treatment system that has been implemented by the City will be considered as meeting the requirement of TRPA Code Chapter 60: Water Quality for drainage storage and treatment. Onsite BMP treatment such as paving and revegetation are still required as part of this program where feasible.

Public and private entities which contribute financially are eligible to participate.

Alternate Traffic and Air Quality Fee Program

The purpose of this program is to implement the transportation improvements depicted on Figure 12 in the Transportation and Circulation Element and to provide a substitute to Subsection 65.2.4.C of the TRPA Code of Ordinances for collection of fees for offsite traffic and air quality mitigation.

Projects contributing to the construction of pedestrian and bicycle infrastructure depicted in Figure 12 will be credited with meeting the necessary mitigation. Fees or assessments paid equivalent to the requirements of TRPA Code Section 65.2.4 to construct the improvements, will be considered in lieu of the requirements of TRPA Code Section 65.2.4.

Public and private entities demonstrating equivalent contributions to the construction of the listed improvements may credit such contributions to TRPA Code Section 65.2.4 requirements.

Land Coverage

Maximum Coverage in a TRPA Designated Town Center

A conforming Area Plan is eligible for the transfer of coverage program pursuant to TRPA Code Section 30.4.2. Maximum coverage for facilities located in the TRPA designated Town Center, as depicted in the Zoning Map (Figure 11), is 70 percent of the project area located within Land Capability Districts 4 – 7, inclusive. Coverage shall be transferred pursuant to TRPA Code Section 30.4.2 and 30.4.3.

Maximum Land Coverage in the Commercial Mixed-Use District

A conforming Area Plan is eligible for the transfer of coverage program pursuant to TRPA Code Section 30.4.2.B.2. Maximum coverage for commercial and mixed-use facilities located in the Commercial Mixed-Use District, as depicted in the Zoning Map (Figure 11) is:

1. For parcels upon which there is no development legally existing as of July 1, 1987, maximum land coverage shall be 70 percent of the project area that is located within Land Capability Districts 4 through 7, inclusive; and
2. For parcels upon which there legally exists development as of July 1, 1987, maximum land coverage shall be 50 percent of the project area that is located within Land Capability Districts 4 through 7, inclusive.
3. Land coverage shall be transferred to eligible parcels and eligible uses, in accordance with TRPA Code Sections 30.4.2 and 30.4.3.

Maximum Coverage in an Alternative Comprehensive Land Coverage Management Program

The purpose of this program is to promote project feasibility, revitalization, coverage reduction and SEZ restoration in the Tahoe Valley area. This system is an alternative to TRPA's land coverage management system in Section 30 of the TRPA Code of Ordinances. Maximum coverage for facilities located in the Town Center Core District, as depicted in the Zoning Map (Figure 11), is 90 percent of the project area located within Land Capability Districts 4 – 7, inclusive. Coverage over the base allowable up to the 70 percent shall be transferred pursuant to TRPA Code Section 30.4.2 and 30.4.3. Coverage over 70 percent up to the maximum of 90 percent shall be transferred using 2.5:1 transfer ratio for coverage from non-sensitive lands, and 1:1 Transfer ratio for coverage from sensitive lands.

Bonus Land Coverage Incentive

The purpose for this program to promote SEZ restoration in the Tahoe Valley Area and the Upper Truckee Hydrologic watershed, all projects transferring or removing coverage from designated sending areas may earn bonus coverage, subject to availability, from the City's coverage pool. Bonus coverage may be earned pursuant to the transfer ratios in the following table:

Sending Area	Bonus Coverage Earned (per sq. ft.)
TRPA Designated Stream Restoration Plan Area (see Figure 9, Zoning Map)	1
Tahoe Valley Greenbelt and Associated SEZs	1
Other SEZs in the Upper Truckee Watershed Hydrologically Related Area Located within the City Limits	0.5

The following land coverage provisions in TRPA Code Section 30.6.4 are applicable throughout the Region:

- Non-motorized public trails are exempt from coverage subject to accessibility, trail route design, trail design, and designation provisions.
- Coverage exemptions for non-permanent structure up to 120 square feet subject to BMPs, land capability verification, location, and aggregate limitation provisions.
- Coverage exemptions for new residential pervious decks subject to size, design, BMPs, location, and aggregate limitation provisions.
- Coverage exemptions for legally existing decks that meet all approval criteria are met.
- Coverage exemptions for compliance with the Americans with Disability Act.

Transfer of Nonconforming Land Coverage in Centers

Pursuant to TRPA Code Subsection 30.4.2.C of the TRPA Code of Ordinances, existing development relocated to a Center where the sending site is restored and retired, may maintain nonconforming land coverage. The receiving site shall be developed in accordance with all applicable TRPA ordinances and the site shall be either the same size or larger than the prior site. If the new project site is smaller, the amount of nonconforming coverage transferred shall be reduced on a proportional basis. The balance of nonconforming coverage may be banked and transferred elsewhere.

Offsite Excess Coverage Mitigation

A project may mitigate its excess land coverage by reducing coverage offsite in a different hydrologically related area provided the restoration occurs on more sensitive lands than the project area.

Onsite Removal and Retirement of Excess Coverage

Pursuant to TRPA Code Section 30.6.3 of the TRPA Code of Ordinances, onsite removal of remaining excess coverage in a Center, may earn multi-residential bonus units, tourist accommodation bonus units, and/or commercial floor area.

Commercial Floor Area Allocation

The City of South Lake Tahoe provides a pool of Commercial Floor Area (CFA) at a reasonable cost and subject to availability in order to create an investment stimulant for creating change. CFA is provided as an incentive, not an entitlement, in order to encourage property owners to move forward with projects that are beneficial to the environment, community and the economy.

Projects eligible for an allocation of CFA from the City's pools must meet the following criteria:

1. Be within an adopted community plan area,
2. Meet the criteria for CFA allocations required by the applicable community plan,
3. Meet all applicable design standards,

4. Include the construction of improvements that will benefit the public and community which are more extensive than required by the applicable community plan or City Code; and
5. Provide an economic benefit to the community with enough significance to merit the allocation of the City's CFA resource.

15,000 square feet of CFA is available for use in Tahoe Valley. CFA can be used to support infill, community-serving retail opportunities, and reuse of existing assets (such as land coverage and CFA), as well as for smaller-scale improvements that add ambiance, diversity, and commercial district vitality and that benefit quality of life.

Approximately 160,347 square feet of CFA is available from the TRPA Pool as development transfer match to Town Centers and the Regional Center. See Transfer of Development Rights to the Tahoe Valley Area Plan and Transfer of Existing Development to the Tahoe Valley Area Plan program descriptions below.

Tourist Accommodation Bonus Units

A total of 82 tourist bonus units are available for use by projects from TRPA's Special Projects Pool in accordance with TRPA Code Section 50.6.4.D.

In addition to the TRPA Code provisions listed above, tourist bonus units may also be earned as a transfer match for the transfer of existing development to Centers in accordance with TRPA Code Section 51.5.3: Transfer of Existing Development to Centers. See Transfer of Existing Development to the Tahoe Valley Area Plan program description below.

Residential Bonus Units

A total of 488 residential bonus units in TRPA's pool are available for use region wide and 600 new residential units are available for use in Centers. Residential bonus units may be earned and used by Projects in accordance with TRPA Code Section 52.3: Multi-Residential Incentive Program of the TRPA Code. Residential bonus units used for affordable housing under this section does not require residential allocations. Residential bonus units used for moderate income housing required an allocation. The City of South Lake Tahoe has a Certified Moderate Income Housing Program pursuant to Section 50.5.2.B and 52.3.6 of the TRPA Code, thus moderate income housing units are eligible to earn allocations from TRPA's unused allocation pool subject to TRPA's allocation procedures.

Pursuant to TRPA Code Section 21.3.2.B, residential bonus units can also be earned under the City's Local Government Housing Program that has been certified by TRPA. The program allows for the conversion of illegal secondary units into deed restricted affordable housing unit.

In addition, residential bonus units may also be earned and used for projects in accordance with TRPA Code Section 51.3.3.C: Transfer of Development Rights to Centers or TRPA Code Section 51.5.3: Transfer of Existing Development to Centers. See Transfer of Development Rights to the Tahoe Valley and Transfer of Existing Development to the Tahoe Valley Area Plan program descriptions below.

Secondary Units

Secondary units are permitted on parcels that are less than one acre in size where single family residential units are permitted.

Transfer of Existing Development to Tahoe Valley

Pursuant to TRPA Code Section 51.5.3, residential bonus units, CFA and tourist bonus unit are available as development transfer match for existing development (residential, tourist accommodation and commercial uses) that is transferred to Centers. Receiving parcels in the plan area may earn residential bonus units, tourist bonus units or CFA at an enhanced transfer ratio based on the land capability of the sending parcels. Transfers of existing residential units are eligible for an additional enhanced transfer ratio based on the sending parcel's distance from Centers and primary transit routes. Residential bonus units earned under this program does not require an allocation to construct a residential unit.

Transfer of Development Rights to Tahoe Valley

Residential bonus units from the TRPA Pool are available as development transfer match for transferring development rights to Centers. Receiving parcels in the plan area may earn residential bonus units at an enhanced transfer ratio based on the land capability of the sending parcel, and its distance from Centers and primary transit routes. Residential bonus units earned under this program do not require an allocation to construct a residential unit. The transferred development right is eligible to receive an allocation from TRPA's unused allocation pool pursuant to Section 51.3.3.C.4 of the TRPA Code.

Persons At One Time

Projects are eligible for the Persons At One Time (PAOT) allocations pursuant to TRPA Code Section 50.9: Regulation of Additional Recreation Facilities of the TRPA Code of Ordinances.

The TRPA Code requires that certain recreation uses and activities are assigned additional PAOTs from TRPA prior to a construction of a project or commencement of an activity. Uses such as marinas, boat launching facilities, information centers, recreation centers, day uses areas, beach recreation, commercial boating, water-oriented outdoor recreation concession, downhill ski facilities, developed campgrounds, group facilities, and recreational vehicle parks are eligible for PAOT allocations from the TRPA Pool.

City of South Lake Tahoe Green Building Program

In November 2010, the City Council adopted the City of South Lake Tahoe Green Building Program (Resolution 2010-92). The program encourages the incorporation of energy efficiency in City buildings and facilities, and provides clear incentives for private property owners to retrofit residential, commercial, and industrial buildings to save energy and reduce the carbon foot print.

Residential projects that incorporate green building measures will be given priority for plan check, by all City Departments, over projects that meet only mandatory requirements. For projects that propose to voluntarily achieve a third party certification such as LEED, Energy Star or Green Point Rated would be eligible for the following:

- Projects would receive priority plan check, over all other projects, by all City Departments.
- Residential projects would have priority on the residential allocation waiting list – 10% of residential allocations would be offered to Green Building projects before other projects on the waiting list.
- Projects would receive recognition at a televised City Council meeting and on the City Website.
- Commercial projects that voluntarily achieve LEED certification or an Energy Star Label would be eligible for the following:
 - Projects would receive priority plan check, over all other projects, by all City Departments.
 - Projects would be eligible for CFA allocation from the City’s unallocated CFA pool.
 - Project would receive recognition at a televised City Council meeting and on the City Website

Change in Use

Commercial changes in use are exempt from project review if the following criteria are met:

1. The change in use is within the commercial use classification;
2. The change in use is to an allowed use;
3. The property is a member of an approved parking assessment district;
4. The change does not increase the parking requirement to a greater requirement than 1 space for 200 square feet., or the number established upon creating the district;
5. The proposed activity is consistent with the Tahoe Valley Area Plan, is not a project by other requirement of TRPA Code Chapter 2: Applicability of the Code of Ordinances of the TRPA Code of Ordinances; and
6. The change in use does not have the potential to intensify pollutant loads over and above the capability of treatment by existing installed BMPs.

Offsite Parking

Use of offsite and shared parking is permissible pursuant to the parking standards in Appendix B of this Plan.

Conversion of Use

Pursuant to TRPA Code Section 50.10: Election of Conversion of Use, the following conversion incentives are available:

- Up to 200 existing Tourist Accommodation Units (TAUs) basin wide can be converted to Existing Residential Units of Use for multi-unit projects subject to the following conditions:
 1. Each converted unit maximum size is 1,250 square feet of residential floor area; and
 2. The conversion must happen on the same parcel.

- Conversion of an existing residential unit of use or tourist accommodation unit to a residential, tourist or commercial use may be permitted when the units are transferred from sensitive lands.
- Conversion of an existing residential unit of use or tourist accommodation units to a residential, tourist, or commercial use may be permitted if the conversion results in the elimination of a non-conforming use.
- Conversion of an existing residential unit to tourist or commercial use or an existing tourist accommodation unit to a commercial use or multi-family unit may be permitted onsite or for transfers if the project meets the TRPA standards applicable for a project proposed on an undeveloped project area.
- Conversion of an existing residential unit of use to a tourist or commercial use or an existing tourist accommodation unit of use to a commercial use or a residential use may be permitted onsite or for transfer if the converted use is include as part of a project that has linked status pursuant to Chapter 15 of the TRPA Code.
- Conversion of existing tourist accommodation units of use to residential may be permitted onsite if the converted units will be used for deed-restricted affordable housing.

APPENDIX A

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APPENDIX B

TRPA Height Findings

Height up to 56 feet in Town Centers and 95 feet in the Regional Center is permitted with the adoption of a conforming Area Plan. To ensure compatibility with adjacent uses, viewshed protection, and mitigation for potentially significant scenic impacts resulting from three- or four-story buildings in the Town Centers and from three- to six-story buildings in the Regional Center, a project shall meet findings 1, 3, 5, and 9 of Section 37.7 of the TRPA Code of Ordinances.

37.7.1 Finding 1

When viewed from major arterials, scenic turnouts, public recreation areas, or the waters of Lake Tahoe, from a distance of 300 feet, the additional height will not cause a building to extend above the forest canopy, when present, or a ridgeline. For height greater than that set forth in Table 37.4.1-1 for a 5:12 roof pitch, the additional height shall not increase the visual magnitude beyond that permitted for structures in the shoreland as set forth in subsection 66.3.7, Additional Visual Magnitude, or Appendix H, Visual Assessment Tool, of the Design Review Guidelines.

37.7.3. Finding 3

With respect to that portion of the building that is permitted the additional height, the building has been designed to minimize interference with existing views within the area to the extent practicable.

37.7.5. Finding 5

The portion of the building that is permitted additional building height is adequately screened, as seen from major arterials, the waters of lakes, and other public areas from which the building is frequently viewed. In determining the adequacy of screening, consideration shall be given to the degree to which a combination of the following features causes the building to blend or merge with the background.

- A. The horizontal distance from which the building is viewed;
- B. The extent of screening; and
- C. Proposed exterior colors and building materials.

37.7.9. Finding 9

When viewed from a TRPA scenic threshold travel route, the additional building height granted a building or structure shall not result in the net loss of views to a scenic resource identified in the 1982 Lake Tahoe Basin Scenic Resource Inventory. TRPA shall specify the method used to evaluate potential view loss.

APPENDIX C

Development and Design Standards

This section sets out the development standards, design standards and permissible uses for the Tahoe Valley Area Plan land use districts in that are designed to achieve the desired community vision and urban form.

Applicability: All projects within the Tahoe Valley Area Plan boundaries shall meet the standards provided below. In addition, all projects are required to meet the City-wide Design Standards when applicable. Should a conflict occur between the City-wide standards and the Tahoe Valley Area Plan standards, the standards of this Area Plan shall apply. All regulations in the TRPA Code of Ordinances shall remain in effect unless superseded by the provisions of this Area Plan.

Permissible Uses

Table 1 lists the permitted primary land uses by district for the Tahoe Valley Area Plan and land use definitions are provided in Table 2. The regulations for the district are established by letter designations listed below. The designations apply strictly to the permissibility of land uses; applications for buildings or structures may require discretionary review. Uses not listed in the Table are prohibited.

(A) designates allowed uses subject to zoning compliance. An allowed use is one that can be established as the primary use of a building.

(S) designates uses that are permitted subject to approval of a Special Use Permit. A Special Use Permit requires discretionary approval by the City Planning Commission or Zoning Administrator following review and a determination that the nature of the proposed use, at the location proposed, is not detrimental to the public welfare or injurious to property or improvements in the neighborhood. To obtain a special use permit, the applicant must generally show that the contemplated use is compatible with the zoning ordinance and land use standards. Findings that such use would be essential or desirable to the public convenience or welfare, and will not impair the integrity and character of the zoned district or be detrimental to the public health, safety, morals or welfare are required.

(TRPA) designates uses that require TRPA review and permitting

(-) designates uses that are not permitted.

**Table 1
PERMITTED USES BY LAND USE DISTRICT**

Permitted Uses Key: "A" – Allowed Use "S" – Special Use "T" – Temporary Use "TRPA" – TRPA Review Required "–" – Use Not Permitted	TC-C	TC-MJC	TC-G	TC-NP	TC-HC	CMX-S	OS
RESIDENTIAL							
Employee Housing	A	A	-	A	A	S	-
Mobile Home Dwelling	-	-	S	S	S	S	-
Multiple Family Dwelling	A	A	A	A	A	S7	-
Multi-Person Dwelling	-	-	-	A	A	-S	-
Nursing and Personal Care	-	-	-	A	A	-	-
Residential Care	-	-	-	A	A	-	-
Single Family Dwelling (includes condominiums)	A2	A2	A2	S	-	S7	-
TOURIST ACCOMMODATION							
Bed & Breakfast Facilities	-	-	A	A	-	-	-
Hotel, Motel, Other Transient Dwelling Units	A	A	A	S	S	-	-
Time Sharing	A	A	A	-	-	-	-
RETAIL COMMERCIAL							
Auto, Mobile Home, and Vehicle Dealers		S		-	-	A	-
Building Material & Hardware	S	A	A	-	-	A	-
Eating & Drinking Places	A	A	A	-	-	-	-
General Retail and Personal Services	A	A	A	-	-	A	-
Nursery	-	S	S	-	-	A	-
Outdoor Retail Sales	-	S	S	-	-	A	-
Service Stations ¹	S	S	S	-	-	A	-
ENTERTAINMENT COMMERCIAL							
Amusement & Recreation	A	A	A	-	-	A	-
Outdoor Amusements	S	S	S	-	-	A	-
Privately Owned Assembly & Entertainment	A	S	S	-	-		-
SERVICE COMMERCIAL							
Animal Services	-	S	S	A	-	A	-
Business Support Services	A	A	A	A	-	A	-
Health Care Services	A	A	A	A	A	A	-
Laundries & Dry Cleaning Plant ¹					-	A	-
Professional Offices	A4	A	A	A	-	A	-
Repair Services ¹	-	A	A	-	A	-	-

**Table 1
PERMITTED USES BY LAND USE DISTRICT**

Permitted Uses Key: "A" – Allowed Use "S" – Special Use "T" – Temporary Use "TRPA" – TRPA Review Required "–" – Use Not Permitted	TC-C	TC-MJC	TC-G	TC-NP	TC-HC	CMX-S	OS
Sales Lot	-	-	-	-	-	A	-
Schools – Business & Vocational	-	S	S	-	S	A	-
Secondary Storage	S	S	S	S	S	A	-
LIGHT INDUSTRIAL COMMERCIAL							
Food & Kindred Products	-	-	-	-	-	A	-
Fuel & Ice Dealers ¹	-	-	-	-	-	A	-
Industrial Services	-	-	-	-	-	A	-
Recycling & Scrap	-	-	-	-	-	A	-
Small Scale Manufacturing	S	S	S	S	-	A	-
Light Manufacturing	-	-	-	-	-	A	-
WHOLESALE/STORAGE COMMERCIAL							
Storage Yards ¹	-	-	-	-	-	A	-
Vehicle & Freight Terminals	-	-	-	-	-	A	-
Parking Lots ¹	S	S	S	S	S	A	-
Warehousing	-	-	S5	-	-	A	-
Wholesale & Distribution	-	-	-	-	-	A	-
GENERAL PUBLIC SERVICE							
Airfields, Landing Strips & Heliports ¹	-	-	-	-	S6	-	-
Collection Stations ¹	-	-	-	-	-	A	-
Cultural Facilities	A	A	A	A	A		-
Daycare Centers/Preschool	A4	A	A	A	A	S	-
Government Offices	A	A	A	A	A	A	-
Hospitals ¹	-	-	-	-	A		-
Local Assembly & Entertainment	A	A	A		-		-
Local Public Health and Safety Facilities ¹	A	A	A	A	A	A	A
Membership Organizations		A	A	A	-	A	-
Post Office	A4	A	A	-	-	-	-
Public Owned Assembly & Entertainment	A	A	A	-	-	-	-
Public Utility Centers ¹	-	-	-	-	-	A	-
Regional Public Health and Safety Facilities	-	-	-	-	A	A	-
Religious Assembly	A	-A	A	A	A	S	-

Table 1 PERMITTED USES BY LAND USE DISTRICT							
Permitted Uses Key: "A" – Allowed Use "S" – Special Use "T" – Temporary Use "TRPA" – TRPA Review Required "- " – Use Not Permitted	TC-C	TC-MJC	TC-G	TC-NP	TC-HC	CMX-S	OS
School – College ¹	-	-	-	S	-	-	-
Schools – Kindergarten through Secondary ¹	-	-	-	S	-	-	-
Social Service Organizations	-	A	A	A	A	A	-
Threshold Related Research Facilities	A4	A	A	A	-	A	-
LINEAR PUBLIC FACILITIES							
Pipelines & Power Transmission	S	S	S	S	S	A	S
Transit Stations & Terminals	S	S	S	S	S	A	
Transmission & Receiving Facilities	S	S	S	S	S	A	S
Transportation Routes	A	A	A	A	A	A	A
RECREATION							
Day Use Areas	A	A	A	A	A	S	A
Outdoor Recreation Concessions	A	S	A	-	-	-	-
Participant Sport Facilities	-	-	-	-	-	S	-
Recreation Centers	-	-	-	-	S	A	-
Riding and Hiking Trails	A	A	A	A	A	A	A
Visitor Information Centers	S	S	S	-	-	-	-
RESOURCE MANAGEMENT							
Forest and Timber Resource Management	A	A	A	A	A	A	A
Vegetation Resource Management	A	A	A	A	A	A	A
Water Quality Improvements and Watershed Management	A	A	A	A	A	A	A
Wildlife and Fisheries Resource Management	A	A	A	A	A	A	A
OPEN SPACE							
Allowed in all areas of the Region	A	A	A	A	A	A	A

Note: In the Town Center all residential projects exceeding 50,000 square feet or non-residential projects exceeding 40,000 square feet require TRPA review and approval.

1. Land use category is identified in TRPA Code Section 60.3 as a "possible contaminating activity," triggering special requirements pursuant to TRPA Code Section 60.4 if located within Source Water Protection Zone (see Figure 9).
2. Condominiums only.

3. Outdoor storage and display is prohibited.
4. As part of a mixed-use project only.
5. Limited to existing warehouse uses within the Town Center Gateway District (TC-G).
Warehouses shall not front on US Highway 50.
6. New non-emergency sites prohibited.
7. Allowed as part of a mixed-used project.

Table 2	
LIST OF PRIMARY USES AND USE DEFINITIONS	
USE	DEFINITIONS
RESIDENTIAL	
Employee Housing	Residential units owned and maintained by public or private entities for purposes of housing employees of said public or private entity.
Mobile Home Dwelling	A home built entirely in the factory on a non-removable steel chassis that is transported to the building site on its own wheels and was installed prior to June 15, 1976, when the Federal Manufactured Home Construction and Safety Standards (commonly known as the HUD Code) went into effect.
Multiple Family Dwelling	More than one residential unit located on a parcel. Multiple-family dwellings may be contained in separate buildings such as two or more detached houses on a single parcel, or in a larger building on a parcel such as a duplex, a triplex, or an apartment building. Vacation rentals are included, up to but not exceeding a four-plex, provided they meet the Local Government Neighborhood Compatibility Requirements as defined in the TRPA Code of Ordinances. One detached secondary residence is included-
Multi-Person Dwelling	A building designed primarily for permanent occupancy by individuals unrelated by blood, marriage, or adoption in other than single-family dwelling units or transient dwelling units. A multi-person dwelling includes, but is not limited to, facilities such as dormitories and boarding houses, but not such facilities as hotels, motels, and apartment houses.
Nursing and Personal Care	Residential establishments with in-patient beds providing nursing and health-related care as a principal use, such as skilled nursing care facilities, extended care facilities, convalescent and rest homes, and board and care homes.
Residential Care	Establishments primarily engaged in the provision of residential social and personal care for children, the aged, and special categories of persons with some limits on ability for self care, but where medical care is not a major element. The use includes, but is not limited to, children's homes, halfway houses, orphanages, rehabilitation centers, and self-help group homes.
Single Family Dwelling (includes condominiums)	One residential unit located on a parcel. A single-family dwelling unit may be contained in a detached building such as a single-family house, or in a subdivided building containing two or more parcels such as a town house condominium. Vacation rentals are included provided they meet the Local Government Neighborhood Compatibility Requirements as defined in the TRPA Code of Ordinances. A caretaker residence is included.
TOURIST ACCOMMODATION	
Bed & Breakfast Facilities	Residential-type structures that have been converted to or constructed as tourist accommodation facilities where bedrooms without individual cooking facilities are rented for overnight lodging, and where at least one meal daily is provided. The use does not include "Hotels and Motels," which are defined separately; nor rooming and boarding houses (see "Multi-Family Dwellings").
Hotel, Motel, Other Transient Dwelling Units	Commercial transient lodging establishments, including hotels, motor- hotels, motels, tourist courts, or cabins, primarily engaged in providing overnight lodging for the general public whose permanent residence is elsewhere. This use does not include Bed and Breakfast Facilities or Vacation Rentals.
Time Sharing	A right to exclusively use, occupy, or possess a tourist accommodation unit, according to a fixed or floating time schedule on a periodic basis occurring annually over a period of time in excess of three years.
RETAIL COMMERCIAL	

**Table 2
LIST OF PRIMARY USES AND USE DEFINITIONS**

USE	DEFINITIONS
Auto, Mobile Home, and Vehicle Dealers	Retail trade establishments selling new and used automobiles, boats, vans, campers, trucks, mobile homes, recreational and utility trailers, motorcycles, golf carts, snowmobile and jet skis (except bicycles and mopeds; see "General Merchandise"). Such businesses are considered a primary use when the establishment sells more than six vehicles per calendar year. The use also includes establishments selling new automobile parts, tires, and accessories (including tire recapping establishments), as well as establishments dealing in used automobiles exclusively. Includes automobile repair shops only when maintained by an establishment selling new vehicles on the same site. Does not include establishments dealing exclusively in used parts (see "Recycling and Scrap") or outside sales (see "Secondary Storage" or "Sales Lots").
Building Material & Hardware	Retail trade establishments within buildings primarily engaged in selling lumber and other building materials, including paint, wallpaper, glass, hardware, nursery stock, and lawn and garden supplies. The use includes all such stores selling to the general public, even if contractor sales account for a larger proportion of total sales. Outside storage or display is included as part of the use. Establishments primarily wholesaling plumbing, heating and air conditioning equipment, and electrical supplies are classified in "Wholesale and Distribution."
Eating & Drinking Places	Restaurants, bars, and other establishments selling prepared foods and drinks for on-premise consumption, as well as facilities for dancing and other entertainment that are accessory to the principal use of the establishment as an eating and drinking place. The use also includes drive-in restaurants, lunch counters, and refreshment stands selling prepared goods and drinks for immediate consumption.
General Retail and Personal Services	An establishment for the retail sale of merchandise or the provision of personal services. A retail facility includes but is not limited to antique or art shops, clothing, drug, dry good, florist, furniture, gift, grocery, hobby, mailing services, office supply, package liquor, paint, pet, shoe, sporting, bike or moped, boats, golf carts, snowmobiles, jet skis, automobile parts, books, toy stores, and other miscellaneous retail shopping goods (auto, mobile home and vehicle sales are not included in this definition). Outdoor display is included as part of this use. A personal service facility includes facilities primarily engaged in providing non-medical services generally involving the care of persons, such as: beauty and barber shops; shoe repair shops; saunas and hot tubs; laundromats (self-service laundries); dry cleaning pick-up stores and small-scale dry cleaners without pick-up and delivery services; clothing rental; dating and escort services;; and wedding chapels. The use may also include the accessory retail sales of products related to the services provided.
Nursery	Commercial retail and wholesale establishment where plants are grown or stored for transplanting at other sites. Outside storage or display is included as part of the use.
Outdoor Retail Sales	Retail trade establishments operating outside of buildings on a daily or weekly basis, such as: roadside stands; flea markets; swap meets; seasonal sales involving Christmas trees, pumpkins, or other seasonal items; regular sales of art or handcrafted items in conjunction with community festivals or art shows; and retail sales of various products from individual motor vehicles locations outside the public right-of-way, not including bakery, ice cream, and similar vending vehicles that conduct all sales within the right-of-way and do not stop

**Table 2
LIST OF PRIMARY USES AND USE DEFINITIONS**

USE	DEFINITIONS
	in any location except on customer demand. Outside storage or display is included as part of the use.
Service Stations	Retail trade establishments primarily engaged in the sale of gasoline, which may also provide lubrication, oil change and tune-up services, and the sale of automotive products incidental to gasoline sales. The use may also include as accessory uses towing, mechanical repair services, car washing and waxing, and trailer rental. The use does not include storage of wrecked or abandoned vehicles, paint spraying body and fender work, and retail sale of gasoline as an accessory use to food and beverage retail sales when limited to not more than two pumps.
ENTERTAINMENT COMMERCIAL	
Amusement & Recreation	Establishments providing amusement or entertainment for a fee or admission charge, such as: arcades and coin-operated amusements; billiard and pool halls; bowling alleys; card rooms; clubs and ballrooms that are principal uses rather than being subordinate to an eating or drinking place; dance halls; gymnasiums; health and athletic clubs; ice skating and roller skating facilities; indoor sauna, spa, or hot tub facilities; motion picture theaters; reducing salons; and tennis, handball, racquetball, indoor archery and shooting ranges, and other indoor sports activities.
Outdoor Amusements	Commercial establishments for outdoor amusement and entertainment such as: amusement parks; theme and kiddie parks; go cart and miniature auto race tracks; and miniature golf courses. Outside storage or display is included as part of the use.
Privately Owned Assembly and Entertainment	Commercially operated facilities for public assembly and group entertainment with a capacity of greater than 300 people, such as: auditoriums; exhibition and convention halls; theaters, meeting halls and facilities for "live" theatrical presentations or concerts by bands and orchestras; amphitheaters; meeting halls for rent; and similar public assembly uses.
SERVICE COMMERCIAL	
Animal Services	Establishments primarily engaged in performing services for animals, such as veterinary services, animal hospitals, animal kennels . The use does not include publicly operated animal control and wildlife care (see "Local Public Health and Safety Facilities").
Business Support Services	Service establishments within buildings that provide other businesses with services including maintenance, repair and service, testing, and rental. This includes establishments such as: outdoor advertising services, mail advertising services (reproduction and shipping); blueprinting, photocopying, and photofinishing; computer-related services (rental, repair, and maintenance); commercial art and design (production); film processing laboratories; and services to structures such as window cleaning, exterminators, janitorial services, and business equipment repair services.
Health Care Services	Service establishments primarily engaged in furnishing medical, mental health, surgical, and other personal health services such as: medical, dental, and psychiatric offices; medical and dental laboratories; outpatient care facilities; and allied health services. Associations or groups primarily engaged in providing medical or other health services to members are included. Nursing homes and similar long-term personal care facilities are classified in "Nursing and Personal Care," and mental health-related services, including various

**Table 2
LIST OF PRIMARY USES AND USE DEFINITIONS**

USE	DEFINITIONS
	types of counseling practiced by licensed individuals other than medical doctors or psychiatrists or unlicensed individuals, are included under Professional Offices.
Laundries & Dry Cleaning Plant	Service establishments primarily engaged in high-volume laundry and garment services, such as power laundries (family and commercial); garment pressing and dry cleaning; linen supply; diaper service; industrial laundries; and carpet and upholstery cleaners. The use does not include coin-operated laundries or dry cleaning pick-up stores without dry cleaning equipment (see "General Retail and Personal Services").
Professional Offices	A place where the following kinds of business are transacted or services rendered: engineering, architectural and surveying; real estate agencies; educational, scientific and research organizations; financial services; writers and artists; advertising agencies; photography and commercial art studios; publishing with offsite printing facilities; broadcasting studios; employment services; off premise concessions (OPC); reporting services; computer services; management, public relations, and consulting services; organizational offices; detective agencies; professional services; attorneys; and counseling services (other than licensed psychiatrists; see "Health Care Services"). Incidental offices are considered accessory uses to a primary use.
Repair Services	Service establishment where repair of consumer products is the principal business activity, such as: electrical repair shops; television, radio, and other appliance repair; watch, clock, and jewelry repair; boat repair; small engine repair; and reupholstery and furniture repair. An outdoor storage yard associated with these uses is considered under "Secondary Storage." The use does not include businesses serving the repair needs of heavy equipment (see "Industrial Services").
Sales Lot	Outdoor sales area for permanent display of motor vehicles, recreational vehicles, mobile homes, construction equipment, farm machinery, or other heavy equipment; outdoor equipment rental yards (not including recreational equipment rental); and large-scale, permanent outdoor sales activities such as livestock auctions and sales. Outside storage or display is included as part of the use.
Schools – Business & Vocational	Business and vocational schools offering specialized trade and commercial courses.
Secondary Storage	The outdoor storage of various materials or the public display of merchandise on the same site as a principal building or use that supports the activities or conduct of the principle use and does not increase the intensity of the use. This does not apply to primary uses that include outside storage and display as part of the use.
LIGHT INDUSTRIAL COMMERCIAL	
Food & Kindred Products	Manufacturing establishments producing or processing foods and beverages for human consumption and certain related products for distribution within the region, such as meat and poultry processing, dairy products processing, beverages and liquors processing, and miscellaneous food preparation from raw products. Outside storage or display is included as part of the use.

**Table 2
LIST OF PRIMARY USES AND USE DEFINITIONS**

USE	DEFINITIONS
Fuel & Ice Dealers	Retail trade establishments primarily engaged in the sale to consumers of ice, bottled water, fuel oil, butane, propane, and liquefied petroleum gas (LPG), bottled or in bulk, as a principal use. Outside storage or display is included as part of the use.
Industrial Services	Service establishments providing other businesses with services, including maintenance, repair, service, testing, publishing, and rental. This includes establishments such as: welding repair, armature rewinding, and heavy equipment repair, vehicle repair, (except vehicle repair; see "Auto Repair and Service"); research and development laboratories, including testing facilities; soils and materials testing laboratories; equipment rental businesses that are entirely within buildings (for equipment rental yards, see "Sales Lots"), including leasing tools, machinery and other business items except vehicles; and other business services of a "heavy service" nature. Outside storage or display is included as part of the use.
Recycling & Scrap	Establishments engaged in assembling, breaking up, sorting, temporary storage, and distribution of recyclable or reusable scrap and waste materials, including auto wreckers engaged in dismantling automobiles for scrap. Outside storage or display is included as part of the use. The use does not include terminal waste disposal sites, which are prohibited and temporary storage of toxic or radioactive waste materials.
Small Scale Manufacturing	Establishments primary engaging in retail sales and secondarily as a fine art or craftsman demonstration workshop of light industrial nature such as sculptor, potter, weaver, carver, jeweler, or other similar art that requires artistic skill . Outside storage or display would require approval of a Special use Permit.
Light Manufacturing	Establishments considered to be light manufacturing or cottage industry that produce jewelry, silverware and plated ware; musical instruments; toys; sporting and athletic goods; pens, pencils, and other office and artists' materials; buttons, costume novelties, miscellaneous notions; brooms and brushes; caskets; and other miscellaneous manufacturing industries. The use also includes artisan and craftsman-type operations that are not home occupations and that are not secondary to on-site retail sales. The use also includes small-scale blacksmith and welding services and the manufacture of trusses. Outside storage or display is included as part of the use.
WHOLESALE/STORAGE COMMERCIAL	
Storage Yard	Service establishments primarily engaged in the business of storing operative cars, buses, or other motor vehicles, construction equipment, materials or supplies, fire wood lots, or industrial supplies. Outside storage or display is included as part of the use.
Vehicle & Freight Terminals	Transportation establishments furnishing services incidental to transportation, such as: freight forwarding services; transportation arrangement services; packing, crating, inspection and weighing services; freight terminal facilities; joint terminal and service facilities; trucking facilities, including transfer and storage; and postal service bulk mailing distribution centers. Outside storage or display is included as part of the use.

**Table 2
LIST OF PRIMARY USES AND USE DEFINITIONS**

USE	DEFINITIONS
Parking Lots	Service establishments primarily engaged in storing operative cars, buses, or other motor vehicles. The use includes both day use and long-term public and commercial garages, parking lots, and structures.
Warehousing	Transportation establishments furnishing services incidental to transportation, such as: freight forwarding services; transportation arrangement services; packing, crating, inspection and weighing services; freight terminal facilities; joint terminal and service facilities; trucking facilities, including transfer and storage; and postal service bulk mailing distribution centers. Outside storage or display is included as part of the use.
Wholesale & Distribution	Establishments engaged in the storage of merchandise for sale to retailers; to industrial, commercial, institutional, farm, or professional business users; or to other wholesalers; or acting as agents or brokers in buying merchandise for or selling merchandise to such persons or companies. The use includes such establishments as: merchant wholesalers; agents, merchandise or commodity brokers, and commission merchants; and assemblers. Outside storage or display is included as part of the use.
GENERAL PUBLIC SERVICE	
Airfields, Landing Strips & Heliports	Transportation facilities that are used for the landing or take-off of aircraft, including helicopters, such as airports, heliports, helipads, and seaplane bases. The use also includes any appurtenant areas used for airport buildings and accessory facilities, including terminals, aircraft sales and rentals, and fueling facilities. Outside storage or display is included as part of the use.
Collection Stations	Establishments engaged in the temporary accumulation and storage of recyclable or discarded materials, including toxic and hazardous wastes, which are subsequently transported to recycling centers or solid waste disposal sites for further processing on a regular and consistent schedule. Outside storage or display is included as part of the use. The use does not include automobile wrecking yards or any recycling processing facilities, which are listed under Recycling and Scrap or regional solid waste transfer stations, which are listed under Recycling and Scrap or Regional Public Health and Safety Facilities.
Cultural Facilities	Permanent public or quasi-public facilities generally of a noncommercial nature, such as art exhibitions, planetariums, botanical gardens, libraries, museums, archives, and arboretums.
Daycare Centers/Preschool	Establishments used for the care of seven or more children residing elsewhere.
Government Offices	Buildings containing office for public agencies, including administrative offices, and meeting rooms that are incidental and accessory to another government use such as transit terminals, vehicle storage, campground, or storage yards.
Hospitals	Establishments primarily engaged in providing diagnostic services and extensive medical treatment, including surgical and other hospital services. Such establishments have an organized medical staff, inpatient beds, and equipment and facilities to provide complete health care.
Local Assembly & Entertainment	Facilities for public assembly and entertainment for the local community, not to exceed a capacity of 300 people, such as community centers, meeting halls, and multi-purpose centers.

**Table 2
LIST OF PRIMARY USES AND USE DEFINITIONS**

USE	DEFINITIONS
Local Public Health and Safety Facilities	Facilities operated by public or quasi-public entities for the local protection of the public, such as: fire stations and other fire prevention facilities; police and sheriff substations; satellite highway maintenance and snow removal facilities; water tanks, pumps, wells and related facilities; monitoring facilities; sewage pumps and related facilities; and emergency services. Outside storage or display is included as part of the use.
Membership Organizations	Permanent meeting facilities for organizations operating on a membership basis for the promotion of the interests of the members, such as: business associations; professional membership organizations; labor unions and similar organizations; civic, social and fraternal organizations; political organizations; and other membership organizations. The use does not include country clubs in conjunction with golf courses (see "Golf Courses"); religious organizations ("see Religious assembly"); and lodging (see "Multi-person Dwelling").
Post Office	Establishments providing mail service and delivery, such as postal substations and neighborhood delivery centers.
Public Owned Assembly & Entertainment	Facilities owned and operated by a public or nonprofit entity for public assembly and group entertainment with a capacity of greater than 300 people, such as: public auditoriums; exhibition and convention halls; civic theaters, meeting halls and facilities for live theatrical presentations or concerts by bands, choirs, and orchestras; meeting halls for rent; community centers; and similar public assembly uses.
Public Utility Centers	Public and quasi-public facilities serving as junction points for transferring utility services from one transmission to another or to local distribution and service, such as: electrical substations and switching stations; major telephone switching centers; natural gas regulating and distribution facilities; public water system wells, treatment plants and storage; and community wastewater treatment plants and settling ponds. Outside storage or display is included as part of the use. The use does not include office or service centers (see "Professional Offices or Government Offices").
Regional Public Health and Safety Facilities	Regional facilities operated by public or quasi-public entities for protection of the public, such as: water and sewage facilities; transportation maintenance/storage facilities; police and sheriff substations and headquarters, including secondary county short-term incarceration facilities; and solid waste transfer stations that TRPA finds to be regionally serving. Secondary county short-term incarceration facility means a county jail (not a state or federal prison facility) that is not the primary jail for the county.
Religious Assembly	Religious organization assembly or institutional facility operated for worship or promotion of religious activities, including churches and incidental religious education. Other establishments maintained by religious organizations, such as full-time educational institutions, hospitals, and other potentially related operations (such as a recreational camp) are not considered a religious assembly and are classified according to their respective activities.
Schools - College	Junior colleges, colleges, universities, and professional schools granting associate arts degrees, certificates, undergraduate and graduate degrees, and requiring for admission at least a high school diploma or equivalent general academic training.
Schools – Kindergarten through Secondary	Kindergarten, elementary, and secondary schools serving grades up to 12, including denominational and sectarian.

**Table 2
LIST OF PRIMARY USES AND USE DEFINITIONS**

USE	DEFINITIONS
Social Service Organizations	Public and quasi-public establishments providing social services and rehabilitation services, counseling centers, welfare offices, job counseling and training centers, or vocational rehabilitation agencies, serving persons with social or personal problems requiring special services and the handicapped and the disadvantaged. The use includes organizations soliciting funds to be used directly for these and related services. The use also includes establishments engaged in community improvement and neighborhood development.
Threshold Related Research Facilities	Public or non-profit research establishments primarily engaged in implementing social, political, and scientific research relating to the Lake Tahoe Environmental Thresholds or the Lake Tahoe ecosystem. The use includes laboratories, monitoring stations, scientific interpretive centers, research and training classrooms, and related support facilities. Overnight multi-person facilities, outside storage, and caretaker facilities may be considered as accessory to this use. The use does not include facilities unrelated to threshold-related research, such as: general college administrative offices and classrooms (see Schools-College); and government administrative offices (see Government Offices and Facilities); or non-threshold-related research (which may be conducted under the Professional Office use).
LINEAR PUBLIC FACILITIES	
Pipelines & Power Transmission	Transportation facilities primarily engaged in the pipeline transportation of refined products of petroleum, such as: gasoline and fuel oils; natural gas; mixed, manufactured, or liquefied petroleum gas; or the pipeline transmission of other commodities. The use includes facilities for the transmission of electrical energy for sale, including transmission and distribution facilities. Outside storage or display is included as part of the use. The use does not include offices or service centers (see "Professional Offices"); equipment and material storage yards (see "Storage Yards"); distribution substations (see "Public Utility Centers"); and power plants (see "Power Generating Plants").
Transit Stations & Terminals	Passenger stations for vehicular and mass transit systems; also, terminal facilities providing maintenance and service for the vehicles operated in the transit system. The use includes, but is not limited to, buses, taxis, railway, and ferries. Outside storage or display is included as part of the use.
Transmission & Receiving Facilities	Communication facilities for public or quasi-public, commercial, and private electronic, optic, radio, microwave, electromagnetic, and photo-electrical transmission and distribution, such as: repeater and receiving facilities, feeder lines, and earth stations for satellite communications for radio, television, telegraph, telephone, data network, and other microwave applications. The use includes local distribution facilities such as lines, poles, cabinets, and conduits. Outside storage or display is included as part of the use. The use does not include uses described under Broadcasting Studios.
Transportation Routes	Public right-of-ways that are improved to permit vehicular, pedestrian, and bicycle travel.
RECREATION	

**Table 2
LIST OF PRIMARY USES AND USE DEFINITIONS**

USE	DEFINITIONS
Day Use Areas	Land or premises, other than Participant Sports Facilities, designated by the owner to be used by individuals or the general public, for a fee or otherwise, for outdoor recreation purposes on a daily basis such as regional and local parks, picnic sites, vista points, snow play areas, rafting facilities, and playgrounds.
Outdoor Recreation Concessions	Facilities that are dependent on the use of outdoor recreation areas, such as onsite food and beverage sales, onsite recreational equipment rentals, parasailing, rafting, and onsite recreation instruction. The use also includes outfitter, guide service establishments, or rental service whose base facilities are located on or near a recreation area, such as horse packing outfitters, snowmobiling outfitters, or bike rental facilities. This use includes offsite rental of sporting equipment whose base facilities is a commercial use. Outside storage or display is included as part of the use.
Participant Sport Facilities	Facilities for various outdoor sports and recreation including, but not limited to, tennis courts, swim and tennis clubs, ice skating rinks, and athletic fields (non-professional). Outside storage or display is included as part of the use.
Recreation Centers	Indoor recreation establishments operated by a public or quasi-public agency providing indoor sports and community services, such as swimming pools, ice skating rinks, multi-purpose courts, weight rooms, and meeting and crafts rooms.
Riding and Hiking Trails	Planned paths for pedestrian, bike, and equestrian traffic, including trail heads.
Visitor Information Centers	Nonprofit establishments providing visitor information and orientation.
RESOURCE MANAGEMENT	
Forest and Timber Resource Management	Activities associated with the protection, restoration, and management of timber such as reforestation, regeneration harvest, sanitation salvage cut, selection cut, special cut, thinning and timber stand improvement.
Vegetation Resource Management	Activities associated with the protection, restoration, and management of vegetation including fire detection and suppression, fuels treatment and management, insect and disease suppression, prescribed fire and burn management.
Water Quality Improvements and Watershed Management	Activities associated with the protection, restoration, and management of watersheds including erosion control, runoff control and stream environment zone restoration.
Wildlife and Fisheries Resource Management	Activities associated with the protection, restoration, and management of wildlife and fisheries resources.
Range Management	Activities associated with range management including range pasture management, range improvement and grazing.
OPEN SPACE	
Allowed in all areas of the Region	Land with no land coverage and maintained in a natural condition or landscaped condition consistent with best management practices, such as deed-restricted properties and designated open space area.

Community Noise Equivalent Level

The maximum community noise equivalent level for this Area Plan is as follows:

TABLE 3 COMMUNITY NOISE EQUIVALENT LEVEL (CNEL)									
DISTRICT	TC-C	TC-MUC	TSC-G	CMX-S	TC-NP	TC-HC	OS	US 50	SR 89
CNEL	65	65	65	65	55		55	65	65

Development Standards

The development standards for the Tahoe Valley Area Plan are intended to provide physical development requirements to implement the Plan’s vision, goals, and policies; implement the goals and policies of the recently adopted General Plan and the Regional Plan Update, and enable the Tahoe Valley Area Plan to be found as a conforming Area Plan by the TRPA Governing Board. The standards were developed based on a review of the TRPA Regional Plan Update policies, the TRPA Area Plans Framework, the City General Plan, the Draft Tahoe Valley Community Plan, and the City of South Lake Tahoe Code and Design Manual.

Form-based coding techniques are used for development standards. The standards of each district list density and lot coverage limits, lot dimensional requirements, building location standards, height requirements, parking and access provisions, and minimum open space and landscaping requirements. Furthermore, substitute design standards address building orientation and design, pedestrian access, bicycle parking. Buildings are required to be oriented toward and include a public entrance on a public street. Building design standards are intended to avoid bulky and “box-like” buildings and encourage appropriate architectural features.

Tables 4 through 9 prescribe the development standards for the Area Plan. Individual letters in parentheses refer to additional regulations that directly follow the table. The numbers in each illustration below refer to corresponding regulations in the “#” column in the associated table.

LOT AND DENSITY

Maximum allowed densities are consistent with the limits set in the Regional Plan Update. The Regional Plan Update allows maximum densities of 25 units per acre for multi-family residential development and 40 units per acre for tourist accommodation with the adoption of an Area Plan. Existing lot size and dimension requirements are proposed to be carried forward. Lot coverage standards reflect the allowances established in the Regional Plan Update, 50 percent within 300 feet of the High Water Mark of Lake Tahoe, 70 percent beyond 300 feet.

1								
DISTRICT	TC-C	TC-MUC	TC-G	TC-NP	TC-HC	CMX-S	OS	
Maximum Density	Mobile Home Dwelling	-	-	8	8	-		
	Multiple Family Dwelling (units/acre)	25	25	25	25	25		
	Multi-Person Dwelling (persons/acre)	-	-	-	25	25		
	Nursing and Personal Care (persons/acre)	-	-	-	25	25		
	Residential Care (persons/acre)	-	-	-	25	25		
	Single Family Dwelling	2 units per parcel, provided one unit is an authorized secondary residence						
	Employee Housing	As per the limitations set forth for residential uses above		-	As per the limitations set forth for residential uses above			
	Bed and Breakfast Facilities (units/acre)	-	-	10	10	-		
	Hotel, Motel, Other Transient Dwelling Units (units/ acre)	40	40	40	40	40		
Time Sharing	40	40	40	-	-	-		
Minimum Lot Size (sq ft)								
Commercial/Public Service	10,000(A)	10,000(A)	10,000(A)	10,000(A)	10,000(A)	10,000(A)		
Residential	6,000(B)	6,000(B)	6,000(B)	6,000(B)	6,000(B)	6,000(B)		
Minimum Lot Width (ft)								
Commercial/Public Service	80(A)	80(A)	80(A)	80(A)	80(A)	80(A)		
Residential	60	60	60	60	60	60		
Minimum Lot Depth (ft)	100(A)	100(A)	100(A)	100(A)	100(A)	100(A)		
Maximum Land Coverage-Base + Transferred (% of project area located within land capability districts 4-7)	The Maximum land coverage shall be 70 percent of the project area that is located within Land Capability Districts 4 through 7, inclusive. (C)					(D)	(E)	
Comprehensive Land Coverage Management System	(F) (G)		(G)					

- A. **Reduced Minimum Lot Size and Dimensions.** Smaller lots may be approved as part of a condominium, or other airspace subdivision pursuant to City Code Section 32-18
- B. **Mobile Home Parks.** Minimum lot size for mobile home parks is 10 acres pursuant to City Code Section 32-17.

C. **Method of Transferring Coverage.** Land coverage shall be transferred to eligible parcels and eligible uses, in accordance with Sections 30.4.2 and 30.4.3 of the TRPA Code.

D. **Commercial and Mixed-Use Facilities in the Commercial Mixed-Use Service District.** The maximum land coverage (base coverage plus transferred coverage) shall be as follows:

1. For parcels upon which there is no development legally existing as of July 1, 1987, maximum land coverage shall be 70 percent of the project area that is located within Land Capability Districts 4 through 7, inclusive; and
2. For parcels upon which there legally exists development as of July 1, 1987, maximum land coverage shall be 50 percent of the project area that is located within Land Capability Districts 4 through 7, inclusive.
3. Land coverage shall be transferred to eligible parcels and eligible uses, in accordance with Section 30.4.3 of the TRPA Code.

Tourist Accommodation Facilities, Multi-Residential Facilities (Five or More Units), Public Service Facilities, and Recreation Facilities. The maximum land coverage (base coverage plus transferred coverage) allowed on a parcel for tourist accommodation facilities, multi-residential facilities of five units or more, public service facilities, and recreation facilities shall be limited to 50 percent of the project area that is located within Land Capability Districts 4 through 7, inclusive. Subdivisions into parcels of four or fewer residential units are not eligible for the maximum land coverage permitted under this subparagraph unless a deed restriction requiring maintenance of the units as affordable or moderate income housing, as defined by TRPA, is approved by TRPA and recorded against the property.

E. **See Chapter 30 of the TRPA Code.**

F. **Comprehensive Land Coverage Management System:** The maximum land coverage (base coverage plus transferred coverage) allowed for facilities located in the Town Center Core District shall be 90 percent of the project area that is located within land capability districts 4-7, inclusive. Land coverage shall be transferred at a ratio of 2.5:1 for coverage from non-sensitive lands, and a ratio of 1:1 for coverage from sensitive lands.

G. **Bonus Coverage Transfer Match** Facilities that transfer or remove coverage from designated sending areas may earn bonus coverage, subject to availability, from the City's coverage pool. Bonus coverage may be earned pursuant to the transfer ratios in the following table:

Sending Area	Bonus Coverage Earned (per sq. ft.)
Stream Restoration Plan Area	1
Tahoe Valley Greenbelt and Associated SEZs	1
Other SEZs in the Upper Truckee Watershed Hydrologically Related Area Located within the City Limits	0.5

BUILDING PLACEMENT

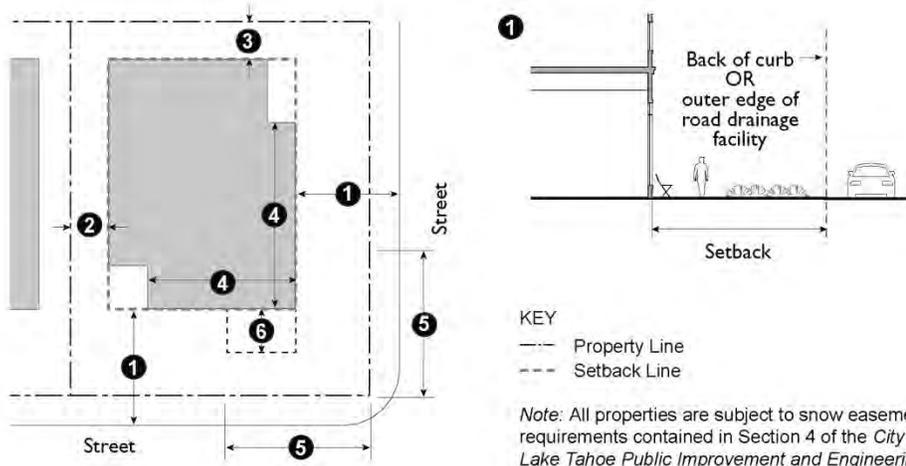
Minimum building setbacks and street frontage improvements are established to create walkable and bikeable districts. Along Highway 50, Lake Tahoe Boulevard, and State Route 89, buildings are required to be setback 25 feet from the back of the curb. The setback area is to consist of a 10 foot sidewalk/use area in which a minimum of six feet clear must be reserved for pedestrian movement and the other area must be improved as part of a wider sidewalk or provide outdoor dining/seating or display area or landscaping.

Along Third Street and Tahoe Keys Blvd, buildings are required to be setback 20 feet. This setback would include a five-foot sidewalk, while the remaining area would be improved as part of a wider sidewalk or provide outdoor dining/seating or display area or landscaping.

Along all other street frontages, with exceptions as noted in the standards, buildings are required to be setback 20 feet.

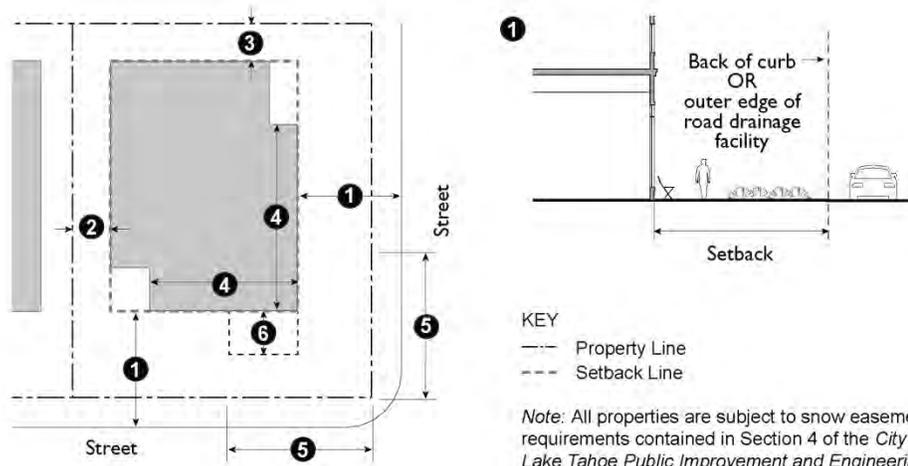
Standards require buildings to be built at this setback line for a certain percentage of the linear street frontage in order to establish a consistent and engaging street frontage and improve the pedestrian experience. Additional regulations require pedestrian-oriented street frontage improvements such as the provision of street trees and lighting. Awnings, overhangs, and other elements to provide a covered walkway, public plaza, or outdoor eating area are allowed to project into the setback area.

**TABLE 5
BUILDING PLACEMENT STANDARDS**



DISTRICT	TC-C	TC-MUC	TC-G	CMX-S	TC-NP	TC-HC	#
Minimum Setbacks (ft.)							
Street Frontage, Hwy 50 and SR 89 and Lake Tahoe Blvd (measured from back of curb or outer edge of road or drainage improvements if no curb)	25' must include a 10 ft. sidewalk/use area with a minimum 6 ft. wide clear sidewalk for pedestrian movement and the balance of the area improved as part of wider sidewalk, outdoor dining/seating area, or landscaping.(A)		25', must include a sidewalk and split rail fence if parking is located between street frontage and a building (A)		-		1
Street Frontage, 3 rd (measured from back of curb or outer edge of road or drainage improvements if no curb)							1
Street Frontage, all other streets (measured from back of curb or outer edge of road or drainage improvements if no curb)			20'(A)				1
Interior Side	Nonresidential Uses: 0; 10' adjacent to residential district				10'		2

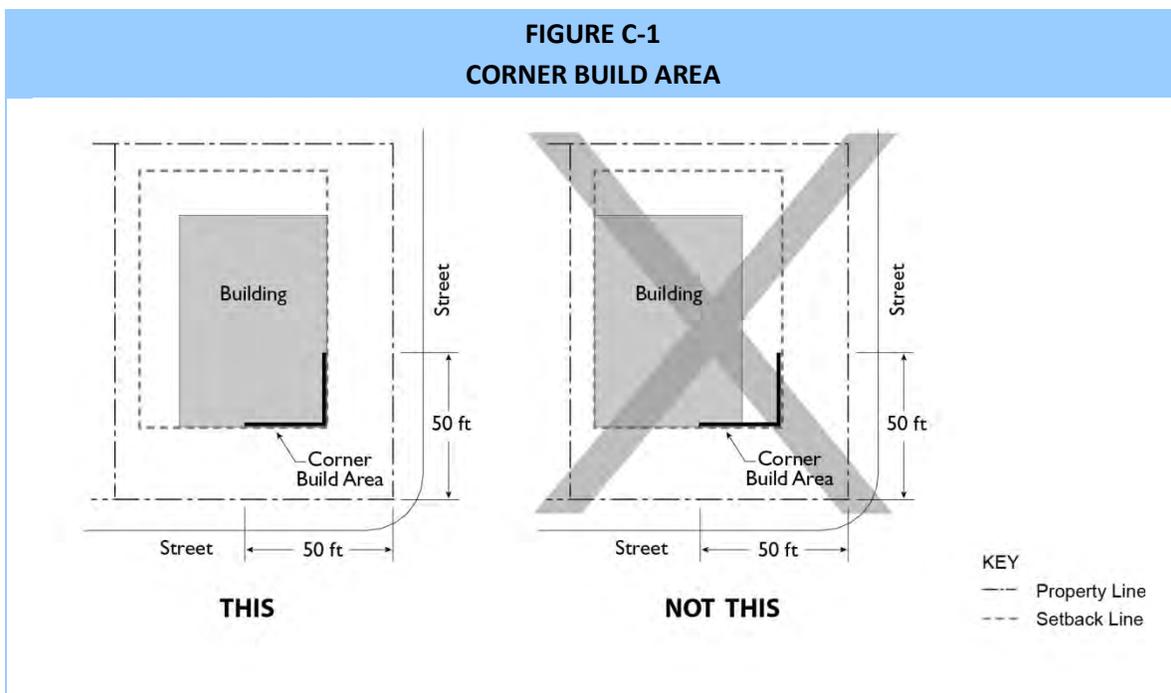
**TABLE 5
BUILDING PLACEMENT STANDARDS**



DISTRICT	TC-C	TC-MUC	TC-G	CMX-S	TC-NP	TC-HC	#
	Residential Uses: 10'						
Rear	Nonresidential Uses: 0; 10' adjacent to residential district Residential Uses: 10'				10'		③
Minimum Building Frontage at Street Setback Line (% of linear street frontage)	75'(B)	50'(B)	-	30'(B)	-		④
Corner Build Area (ft)	50'(C)	50'(C)	-	-	-		⑤
Maximum Projection into Setback	Awnings and overhangs to provide a covered walkway, public plaza, or outdoor eating area may project up to 10 ft. into the required setback.				n/a		⑥

- A. **Street Frontage Improvements.** New development shall provide street frontage improvements in accordance with the following:
1. **Street Trees.** A minimum of two trees and four shrubs shall be planted for every 50 linear feet of street frontage.
 2. **Lighting.** Pedestrian scaled lighting on sidewalks and pathways consistent with City pedestrian lighting standards. Lighting standards that include attachments from which banners identifying the area or announcing community events may be hung are encouraged.
 3. **Alternatives through Director Review.** Alternatives to the minimum setback requirement for the four parcels located at the South “Y” intersection may be approved if the Development Services Director finds that:

- a. The increased setback is needed to maintain a sun angle plane to reduce icing on US 50, SR 89 and public sidewalks, or
 - b. The increased setback is needed to maintain visual open space, visual access, or preserve or increase views of the mountain ridgelines.
- B. **Build-to Line.** Buildings shall be constructed at the required setback for the percent of linear street frontage identified in Table 5. This requirement may be modified or waived by the Director of Development Services upon finding that:
- 1. Entry courtyards, plazas, entries, or outdoor eating areas are located between the build-to line and building, provided that the buildings are built to the edge of the courtyard, plaza, or dining area; or
 - 2. The building incorporates an alternative entrance design that creates a welcoming pedestrian entry feature facing the street.
- C. **Corner Build Area.** Buildings must be located at the point of intersection of the two setback lines at street corners, and must occupy the street frontage at the setback line for at least 50 feet from the street corner property line. Public plazas may be at the street corner provided buildings are built to the edge of the public plaza. Where a parcel is bounded by more than two streets, this requirement only applies to the primary street frontage and one side street frontage. This requirement may be modified or waived by the Director of Development Services upon finding that:
- 1. Where buildings placed on a corner would negatively impact a designated scenic view, visual open spaces, visual access or views of mountain ridgelines.; or
 - 2. Where building placed on a corner would negatively affect the sun angle plane to reduce icing on US 50, SR 89 and public sidewalks, or

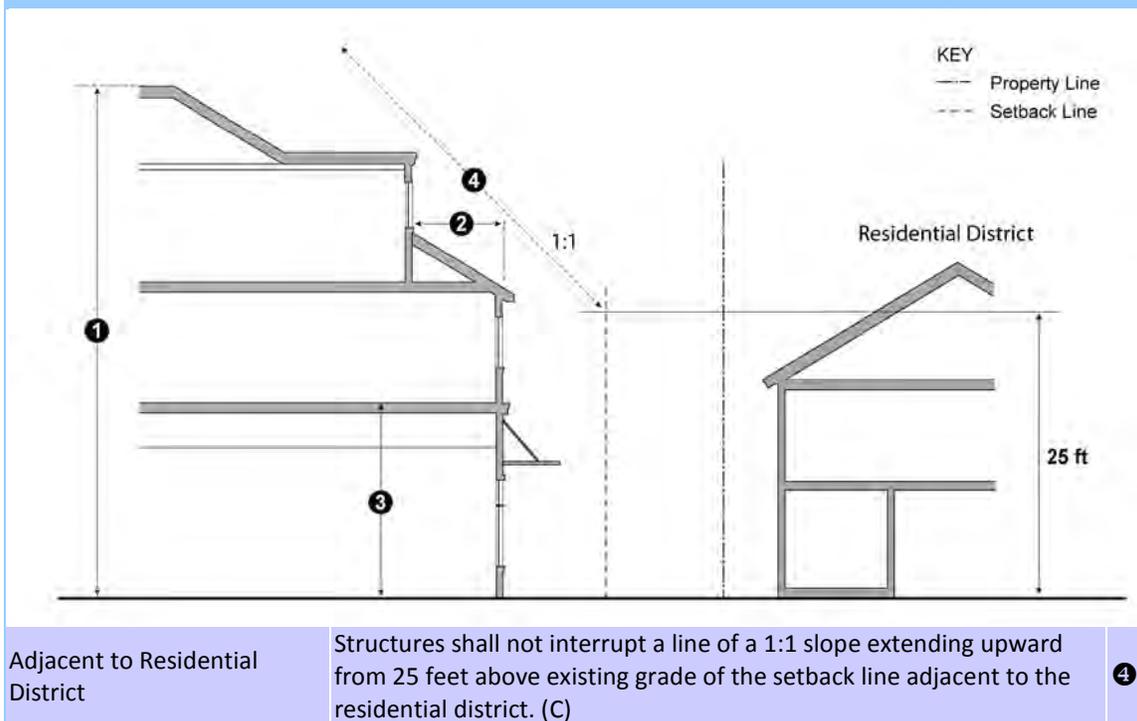


HEIGHT

Maximum height limits proposed in the Regional Plan Update have been refined to reflect the nature of development in each sub district. Buildings are required to step back within a daylight plane along street frontages and adjacent to residential areas to address massing, protect viewsheds, allow light and air, and limit winter shading in public areas. Minimum heights are proposed for ground floors in order to establish a consistent building frontage along the street and to accommodate a wide range of uses. Roofs are required to be sloped to support alpine character in design. Limited areas of flat roofs are allowed as long as they are not visible from the public right-of-way.

TABLE 7 HEIGHT AND ROOF STANDARDS							
DISTRICT	TC-C	TC-MUC	TC-G	CMX-S	TC-NP	TC-HC	#
Building Height Maximum (feet)	45'	42'	36'	See TRPA Code Section 37.4	36'	42'	①
Building Height Maximum (stories)	3(A)	3(A)	2(A)	-	2(A)	3(A)	①
Building Stepbacks							
Upper Story along Street Frontage (ft.), applicable above the second story	10' from second story building face			-	-		②
Ground Floor Minimum Height, Non-Residential Uses (ft)	15'	15'	15'		-		③
Roof Slope	5:12 to 12:12 (B)						
Roof Height	The height of the sloped roof must be a minimum 40% of the height of the building. (B)				-		

**TABLE 7
HEIGHT AND ROOF STANDARDS**



- A. **Viewshed Protection.** To ensure compatibility with adjacent uses and viewshed protection, buildings must not project above the forest canopy, ridge lines, or otherwise detract from the viewshed and the review authority must make findings 1, 3, 5, and 9 of Section 37.7 of the TRPA Code of Ordinances in approving any project consisting of three or more stories (see Appendix B).
- B. **Roof Design.** Buildings shall have a definitive “top” that steps, slopes, or otherwise breaks the rectangular form of the building. Dormer windows and other roof appurtenances (chimneys, towers, or other special features) are encouraged within the sloping roof area. A portion of the roof area may be flat. Flat roof area is limited to 50 percent of the building footprint and shall not be within the predominant view of the public right-of-way. Covered parking and parking structures are exceptions and may be permitted to have flat roofs, but must be concealed through architectural design or landscaping. Rooftop equipment shall be concealed from view and/or integrated within the building architecture.
- C. **Transition Height.** To ensure compatibility with adjacent residential uses, buildings that are permitted additional height shall have additional buffering in addition to the required setback. Provisions may include, but are not limited to the following:
- Reduced height
 - Increase side yard or rear yard setback
 - Building orientation

- Landscaping buffering with oversized trees

BUILDING FORM

Building form standards limit blank walls, require a minimum amount of building transparency (i.e. windows and doors providing views into work areas, display areas, sales areas, or similar active spaces) along street frontages, and require the modulation of building facades to create visual interest and engage pedestrians.

TABLE 8 BUILDING FORM STANDARDS						
<p>KEY Building Wall Area</p>						
DISTRICT	TC-C	TC-MUC	TC-G	CMX-S	TC-NP	TC-HC #
Maximum Length of Blank Wall	25(A)	25(A)	25(A)	-	-	①
Required Transparency (% of building wall area)	75(A)	75(A)	60(A)	-	-	②
Building Modulation	Any building over 50 feet wide shall be broken down to simulate a series of buildings no wider than 50 feet each				-	

A. **Building Transparency and Blank Wall Limits; Required Openings for Non-Residential Uses.**

Exterior walls facing and within 20 feet of a front or street side property line or pedestrian walkway shall include windows, doors, or other openings for at least the percentage stated in Table 8 of the building wall area located between 2.5 and seven feet above the level of the sidewalk or adjacent ground grade. No wall may run in a continuous horizontal plane for more than 25 feet without an opening or transparency on the ground floor of a building.

1. **Design of Required Openings.** Openings fulfilling this requirement shall have transparent glazing and provide views into work areas, display areas, sales areas, lobbies, or similar active spaces, or into window displays that are at least three feet deep.
2. **Exceptions for Parking Garages.** Multi-level garages are not required to meet the building transparency requirement of this subsection. Instead, they are subject to the building setback standards in Table 5, Building Placement Standards and the following standard:

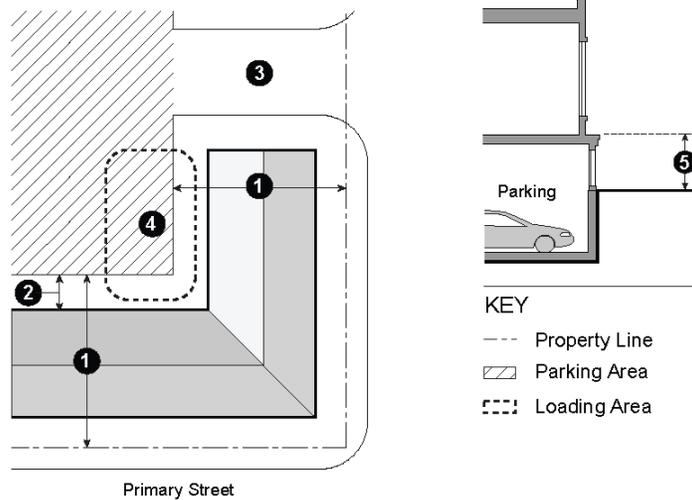
- a. *Parking Garage Rooftop Planting.* Uncovered parking on the top level of a parking structure shall have rooftop planters with a minimum dimension of 24 inches along perimeter walls of the top floor that face public streets or other public areas.
3. **Alternatives through Director Review.** Alternatives to the building transparency requirement may be approved if the Development Services Director finds that:
 - a. The proposed use has unique operational characteristics with which providing the required openings is incompatible, such as in the case of a cinema or theater; and
 - b. Street-facing building walls will exhibit architectural relief and detail, and will be enhanced with landscaping in such a way as to create visual interest at the pedestrian level.

PARKING, LOADING, LANDSCAPING AND OPEN SPACE

The proposed parking and loading location standards are intended to enhance walkability and reduce the visual dominance of surface parking lots in the area by setting them back from the street. Parking in each district is required to be setback from the back of curb with access from a side street or alley wherever possible. Limitations on the number of curb cuts and driveway widths from the South Tahoe Redevelopment Demonstration Plan have been carried forward. To support a “park once” atmosphere where visitors park once and patronize multiple businesses, new parking facilities are to be designed to accommodate cross-access to/from adjacent properties to allow parking areas to become joint use facilities even if initially serving only one development.

Standards are established for the minimum amount of open space and landscaping. Residential open space is required in all residential and mixed-use development. A key component of the vision for the Tahoe Valley area is to provide areas for public gathering. To implement this vision, public open space is required with substantial developments (developments over 50,000 square feet of nonresidential floor area on sites greater than two acres in size) in the TSC-C district.

**TABLE 9
PARKING, LOADING, LANDSCAPING, AND OPEN SPACE STANDARDS**



DISTRICT	TC-C	TC-MUC	TC-G	CMX-S	TC-NP	TC-HC	#
Minimum Setback from Street Property Line (ft.)	25(A), Parking shall be located underground, behind a building, or on the interior side or rear of the site.			20(A)	20 (A)		①
Setback from Buildings and Public Plazas (ft.)	Above ground parking shall be setback from buildings and public plazas with a walkway and/or landscaping.			-	-		②
Access Location	Side street or alley wherever possible			From adjacent parking areas wherever possible			③
Shared Access	See additional regulations (B)						③
Curb Cuts	Minimized and in areas least likely to affect pedestrian circulation.						
Loading/Service Areas	Side or rear of lot; must be screened from public ROW						④
Underground Parking Visibility	Maximum height of underground parking area visible from the street (parking podium) is 3 feet from finished grade.						⑤
Parking, Driveway and Loading Standards	See South Lake Tahoe City Code, Chapter 5, Land Use Development Standards, Article VIII. Parking, Driveway and Loading Spaces.						
Minimum Residential Open Space (sq. ft. per unit)	100(C)	100(C)	100(C)	-	150(C)		
Minimum Public Open Space, sites two acres or more in size	(D)	-	-	-	-		
Minimum Amount of Landscaping (% of site)	10	10	10	5	10		

A. **Limitations on Location of Parking.** Parking may be located within the required setback, subject to the following requirements.

1. ***Underground and Partially Submerged Parking.*** Parking completely or partially underground, may be located within the required setback.
 2. ***Surface Parking.*** Above ground surface parking may be located within the required setback with Development Services Director approval provided:
 - a. Buildings are built close to the public sidewalk to the maximum extent feasible;
 - b. The parking area is screened along the public right-of-way with public art, hedge, trellis, and/or landscaping; and
 - c. The site is small and/or constrained such that locating parking outside the required setback is not feasible.
- B. **Shared Access.** To encourage shared parking and shared access points on public streets, new parking facilities shall be designed to accommodate cross-access to/from adjacent properties to allow parking areas to become joint use facilities even if initially serving only one development. When cross-access for vehicles is deemed impractical by the Development Services Director, the requirement for cross-access may be waived if bicycle and pedestrian connections are provided between adjacent development.
- C. **Residential Open Space.** Residential open space may be common or private open space. Private areas typically consist of balconies, decks, patios, fenced yards, and other similar areas outside the residence. Common areas typically consist of landscaped areas, walks, patios, swimming pools, barbecue areas, playgrounds, turf, or other such improvements as are appropriate to enhance the outdoor environment of the development. Landscaped courtyard entries that are oriented towards a public street are considered common areas. All areas not improved with buildings, parking, vehicular accessways, trash enclosures, and similar items shall be developed as common areas.
1. ***Minimum Dimensions.***
 - a. ***Private Open Space.*** Private open space located on the ground level (e.g., yards, decks, patios) shall have no horizontal dimension less than 10 feet. Private open space located above ground level (e.g., balconies) shall have no horizontal dimension less than six feet.
 - b. ***Common Open Space.*** Minimum horizontal dimension of 20 feet.
 2. ***Usability.*** A surface shall be provided that allows convenient use for outdoor living and/or recreation. Such surface may be any practical combination of lawn, garden, flagstone, wood planking, concrete, or other serviceable, dust free surfacing. The maximum slope shall not exceed 10 percent. Seating areas and plazas should be located in areas with good solar exposure.
 3. ***Accessibility.***
 - a. ***Private Open Space.*** The space shall be accessible to only one living unit by a doorway to a habitable room or hallway.

- b. *Common Open Space.* The space shall be accessible to all the living units on the lot.
- D. **Public Open Space.** Developments with 50,000 square feet or more of nonresidential floor area on sites two acres or more in size shall provide public open space according to the following standards:
 1. **Minimum Area.** Forty square feet for every 1,000 square feet of nonresidential floor area for the first 100,000 square feet of floor area, plus 20 square feet for every 1,000 square feet of nonresidential floor area over 100,000 square feet.
 2. **Minimum Dimensions.** Minimum horizontal dimension of 40 feet.
 3. **Usability.** A surface shall be provided that allows convenient use for outdoor living, recreation, and public gathering. Such surface may be any practical combination of high quality plant and hardscape materials such as bricks, stone, concrete, permeable paving, or tile. Surfaces shall be sloped for positive drainage and constructed to withstand snow removal and de-icing maintenance. The maximum slope shall not exceed 10 percent. Seating areas and plazas should be located in areas with good solar exposure.
 4. **Accessibility.** On-site public space shall be visible from a public street and from on-site areas normally frequented by nearby retail uses.
 5. **Amenities.** On-site public space shall include benches or other seating. Amenities shall be included that enhance the comfort, aesthetics, or usability of the space, including but not limited to trees and other landscaping, shade structures, drinking fountains, water features, public art, trash receptacles, information kiosks, or performance areas.

Substitute Design Standards

Substitute Design standards address site design and amenities, building orientation and design, pedestrian access, bicycle parking, landscaping, lighting, signage, parking, driveway and loading spaces.

Buildings are required to be oriented toward and include a public entrance on a public street. Building design standards are intended to avoid bulky and “box-like” buildings and encourage appropriate architectural features.

On-site access and circulation standards require a system of pedestrian walkways that connect all buildings on a site to each other, to on-site parking areas, and open space or pedestrian amenities. Regular connections are to be provided to the public sidewalk as well as to adjoining commercial and residential areas. Walkway standards establish requirements for minimum width, paving, grade separations, bollards, landscaping, lighting, or other means to clearly delineate pedestrian areas for both day and night use.

Standards are for the amount, design, and location of bicycle parking is incorporated. The standards have different requirements for long- and short-term bicycle parking that reflect the varying needs of visitors, locals, and employees.

Landscape standards reinforce the resort destination experience in the Area Plan by guiding the desired design of planting and hardscape materials while providing for opportunities to accommodate stormwater runoff.

Lighting standards for streets and exterior spaces is provided that are necessary to provide safety and security as well as provide, in limited areas, the night lighting that will allow for a festive atmosphere enhancing the qualities of an active civic place.

All projects within the Tahoe Valley Area Plan shall meet the standards listed below and the standards in the Citywide Design Standards and the Parking, Driveway and Loading Space Standards in Chapter 5 of the City Code and the Sign Standards in Chapter 25 of the City Code. Should a conflict occur between the standards of this Area Plan and the standards of the City Code, the standards of the area plan shall apply. All regulations in the TRPA Code of Ordinances shall remain in effect unless superseded by the provisions of this Area Plan.

A. Site Design and Amenities.

1. Site design shall consider adjoining properties to avoid creating nuisances such as noise, light intrusion, invasion of privacy, and traffic, particularly when development is adjacent to sensitive uses such as residential development.
2. Outdoor parking areas shall be designed to decrease visual impacts associated with large expanses of pavement and vehicles. They can be divided into smaller parking areas and/or include landscaping within and around them. Consider using perimeter landscaping and landscaped islands as bio-swales with a lower grade than the paved surface to reduce irrigation requirements and meet stormwater retention requirements.
3. Site design shall consider pedestrian safety during snow conditions. Sidewalks, plazas and other pedestrian areas should be designed for maximum solar exposure, with snow melt systems, or for efficient snow removal.
4. Trash and recycling receptacles shall be provided and conveniently located in areas with heavy pedestrian use.
5. Ski and snowboard racks shall be provided in areas where there will be a need to temporarily store them. Racks should be located at areas adjacent to ski related facilities, public areas, transit stops, and destination locations.
6. For developments with multiple commercial or service tenants, include directional/directory maps on-site to orient and direct pedestrians.

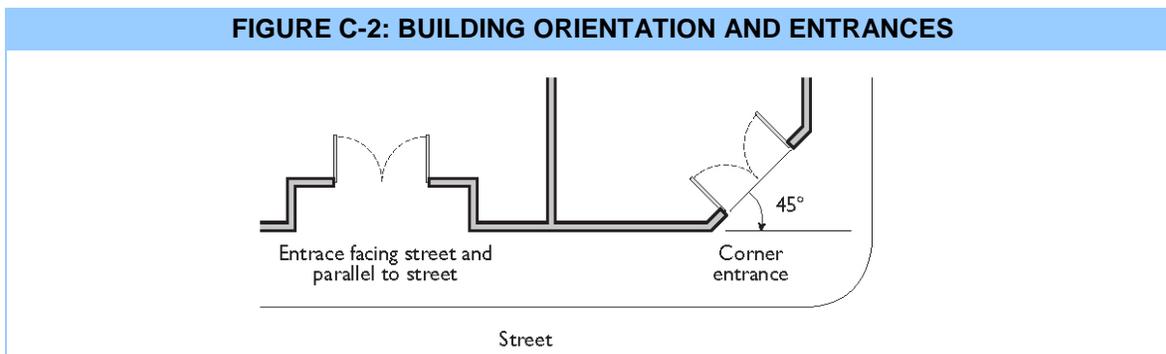
B. Building Orientation and Entrances.

1. Primary building entrances for all buildings shall be located facing a public street.
2. Building entrances shall be emphasized with special architectural and landscape treatments, shall provide covered overhead protection in the form of recessed arcades or protruding canopies, and shall ensure protection from unloading roof snow.
3. Entrances located at corners shall generally be located at a 45 degree angle to the corner and shall have a distinct architectural treatment to animate the

intersection and facilitate pedestrian flow around the corner. Different treatments may include angled or rounded corners, arches, and other architectural elements. All building and dwelling units located in the interior of a site shall have entrances from the sidewalk that are designed as an extension of the public sidewalk and connect to a public sidewalk.

4. Entrances to residential units shall be physically separated from the entrance to the permitted commercial uses and clearly marked with a physical feature incorporated into the building or an appropriately scaled element applied to the façade.

FIGURE C-2: BUILDING ORIENTATION AND ENTRANCES



C. **Building Design and Articulation.** Buildings shall provide adequate architectural articulation and detail to avoid a bulky and “box-like” appearance.

1. **Exterior Building Materials and Colors.**

- a. A unified palette of quality materials shall be used on all sides of buildings.
- b. Colors shall be used to help delineate windows and other architectural features to increase architectural interest.
- c. A variety of natural-appearing materials should provide contrast on building facades.
- d. Colors should be chosen to blend in with the setting and to minimize reflectivity. Bright colors should be used for accent only and should be applied to a maximum of 10 percent of the building façade.
- e. Roofs, including mechanical equipment and skylights, shall be constructed of non-glare finishes and earth tone colors that minimize reflectivity.

2. **Building Details.** Buildings shall provide adequate architectural articulation and detail to avoid a bulky and “box-like” appearance.

- a. Building façades shall include building projections or recesses, doorway and window trim, textured materials, differentiated piers and columns,

awnings, and other details that provide architectural articulation and design interest.

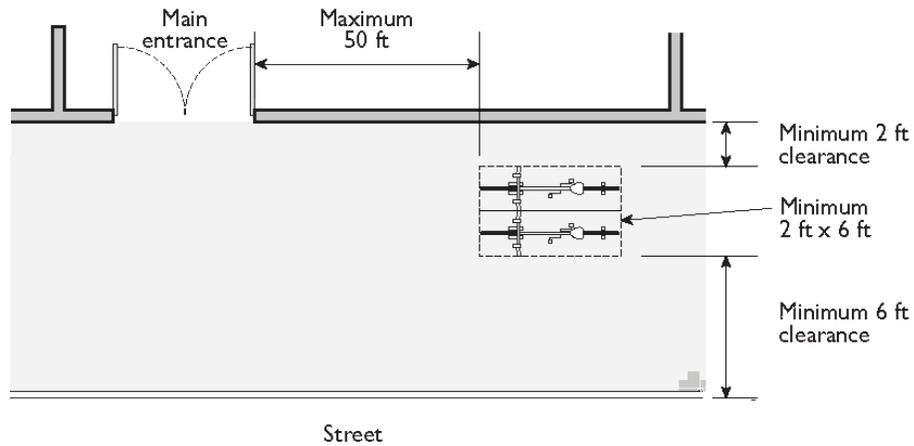
- b. Cornices, balconies, roof terraces, and other architectural elements should be used, as appropriate, to terminate rooflines and accentuate setbacks between stories.
 - c. All applied surface ornamentation or decorative detailing shall be consistent with the architectural style of the building.
 - d. Each side of the building that is visible from a public right-of-way shall be designed with a complementary level of detailing. Particular attention shall be given to the detailing within the pedestrian's range of touch and view, such as the use of special store-front detailing and façade ornamentation to reinforce the pedestrian character of the street.
3. **Green Building.** New buildings and retrofits to existing buildings are long term investments and should feature quality design/materials, flexible design to deal with changing demands and be built green. The City encourages residential, commercial and industrial properties to consider incorporating green building measures. Property owners who participate in the City Green Building Incentive Program (See Appendix D) are eligible for the following incentives:
- a. Projects will receive priority plan check by City Departments.
 - b. Residential projects will receive priority on the residential allocation waiting list.
 - c. Commercial projects area eligible for CFA allocations form the City's CFA pool.
 - d. Projects will receive recognition by the City Council.
- D. **Pedestrian Orientation and Access.** On-site pedestrian circulation and access must be provided according to the following standards.
1. **Internal Connections.** A system of pedestrian walkways shall connect all buildings on a site to each other, to on-site automobile and bicycle parking areas, and to any on-site open space areas or pedestrian amenities.
 2. **To Circulation Network.** Regular connections between on-site walkways and the public sidewalk shall be provided. An on-site walkway shall connect the primary building entry or entries to a public sidewalk on each street frontage.
 3. **To Neighbors.** Direct and convenient access shall be provided from commercial and mixed-use projects to adjoining residential and commercial areas to the maximum extent feasible while still providing for safety and security.
 4. **To Transit.** Safe and convenient pedestrian connections shall be provided from transit stops to building entrances.
 5. **Interior Pedestrian Walkway Design.**

- a. Walkways shall have a minimum clear unobstructed width of six feet, shall be hard-surfaced, and paved with concrete, stone, tile, brick, pervious paving, or comparable material.
- b. Where a required walkway crosses driveways, parking areas, or loading areas, it must be clearly identifiable through the use of a raised crosswalk, a different paving material, or similar method.
- c. Where a required walkway is parallel and adjacent to an auto travel lane, it must be raised or separated from the auto travel lane by a raised curb at least four inches high, bollards, or other physical barrier.

E. Bicycle Parking.

- 1. **Short-Term Bicycle Parking.** Short-term bicycle parking shall be provided in order to serve shoppers, customers, messengers, guests and other visitors to a site who generally stay for a short time.
 - a. *Parking Spaces Required.* The number of short term bicycle parking spaces shall be at least 10 percent of the number of required automobile parking spaces, with a minimum of two spaces per establishment, for commercial, mixed-use, and multi-family structures.
 - b. *Location.* Short-term bicycle parking must be located within 50 feet of a main entrance to the building it serves. Bicycle parking shall be visible from the street or from the main building entrance, or a sign must be posted at the main building entrance indicating the location of the parking. Bicycle parking shall be located outside of the public right of way except with an encroachment permit in the TSC-C and TSC-MUC districts, provided an unobstructed sidewalk clearance of six feet is maintained for pedestrians at all times.
 - c. *Anchoring and Security.* For each short-term bicycle parking space required, a stationary, securely anchored object shall be provided to which a bicycle frame and one wheel can be secured with a high-security U-shaped shackle lock if both wheels are left on the bicycle. One such object may serve multiple bicycle parking spaces.
 - d. *Size and Accessibility.* Each short-term bicycle parking space shall be a minimum of two feet in width and six feet in length and shall be accessible without moving another bicycle. Two feet of clearance shall be provided between bicycle parking spaces and adjacent walls, poles, landscaping, street furniture, drive aisles, and pedestrian ways and at least five feet from vehicle parking spaces.

**FIGURE C-3
SHORT-TERM BICYCLE PARKING**



2. **Long-Term Bicycle Parking.** Long-term bicycle parking shall be provided in order to serve employees, residents, commuters, and others who generally stay at a site for four hours or longer.
 - a. **Parking Spaces Required.**
 - i. **Residential Uses.** A minimum of one long-term bicycle parking space shall be provided for every five units for multi-unit residential and group residential projects.
 - ii. **Parking Lots and Structures.** Long-term bicycle parking shall be provided at a minimum ratio of one space per 50 vehicle spaces.
 - b. **Location.** Long-term bicycle parking must be located on the same lot as the use it serves. In parking garages, long-term bicycle parking must be located near an entrance to the facility.
 - c. **Covered Spaces.** At least 50 percent of required long-term bicycle parking must be covered. Covered parking can be provided inside buildings, under roof overhangs or awnings, in bicycle lockers, or within or under other structures.
 - d. **Security.** All long-term bicycle parking must be located in a secure area such as the following:
 - i. An enclosed bicycle locker;
 - ii. A fenced, covered, locked or guarded bicycle storage area;
 - iii. A rack or stand inside a building that is within view of an attendant or security guard or visible from employee work areas; or
 - iv. Other secure area approved by the Development Services Director.

- e. *Size and Accessibility.* Each bicycle parking space shall be a minimum of two feet in width and six feet in length and shall be accessible without moving another bicycle. Two feet of clearance shall be provided between bicycle parking spaces and adjacent walls, poles, landscaping, street furniture, drive aisles, and pedestrian ways and at least five feet from vehicle parking spaces.
- F. **Snow Storage: See SLCC Article VI, City-Wide Design Standards Section 5-23.**
 - G. **Landscape Design: See SLCC Article VI, City-Wide Design Standards Section 5-24.**
 - H. **Exterior Lighting: See SLCC Article VI, City-Wide Design Standards Section 5-25.**
 - I. **Water Conservation: See SLCC Article VI, City-Wide Design Standards Section 5-26.**
 - J. **Street Right-of-Way Improvements:** The improvement of the public right-of-way is necessary for the safety and convenience of the residents, improving the quality of the community and the lake and unifying the appearance of the community with the establishment of a consistent set of improvement standards.
 - 1. Standard: All new road construction projects or the repair of existing improvements within the publicly owned right-of-way shall comply with the City of South Lake Tahoe Public Improvement Engineering Standards.
 - 2. Standard: Road fixtures, including but not limited to retaining walls, safety barriers, traffic signals and controllers, light standards, and other structures, shall be designed and installed in compliance with the with the City of South Lake Tahoe Public Improvement Engineering Standards.
 - 3. Standard: Color of road fixtures (other than signals, their auxiliary equipment and sign standards). Refer to the City of South Lake Tahoe Public Improvement Engineering Standards for specific details.
 - 4. Standard: Signal poles and their ancillary equipment and sign structures. Refer to the City of South Lake Tahoe Public Improvement Engineering Standards for specific details.
 - 5. Standard: Retaining walls and other erosion control devices. Refer to the City of South Lake Tahoe Public Improvement Engineering Standards and Chapter 36, City Grading Ordinance, for specific details.
 - 6. Standard: Street improvements required. Public and private projects shall be required to construct off-site improvements including curb, gutter, sidewalk and other improvements required by the city engineering division, consistent with the standards contained within the City of South Lake Tahoe Public Improvement Engineering Standards, including street right-of-way improvements and landscaping standards, SLTCC 5-24, or at the discretion of the city engineer provide in-lieu fees when:
 - a. A Project located in the TC-C, TC-G, and TC-MUC that involves a vacant lot and/or is a demolition and reconstruction project, or redevelopment

or remodel with construction valuation of 50% or more than the structure valuation construction;

- i. The project is processed as a special use permit and is determined by the city engineering division that the off-site improvements are necessary for the orderly development of the area;
- ii. A public project, not subject to the special use permit process, is determined by the city engineering department to need off-site improvements for the orderly development of the area. (Ord. 903; Ord. 1002)

- K. **Scenic Highway Corridors: See SLCC Article VI, City-Wide Design Standards Section 5-28.**
- L. **Shorezone:** The shorezone is regulated by the TRPA Code, Chapters 80 through 86, and not reiterated here. As a result, all projects which fall within this area shall be referred to the TRPA for review. The city's review will be limited to providing input into the TRPA process and processing the project through the city permit process.
- M. **Parking, Driving and Loading: See SLCC Chapter 5, Article VIII.**
- N. **Signage: See SLCC Chapter 25.**

APPENDIX D

CSLT Green Building Program

RESIDENTIAL PROJECTS

Level 1 – “Priority Plan Check”

For residential projects, including new construction, additions and remodels, City staff has drafted a list of “Recommended Green Measures for Residential Projects.” These measures are feasible to implement and do not require significant additional investment beyond the mandatory measures of building codes and the CalGreen Code. Additionally, they have a good “bang for the buck” and the City would like to encourage applicants to incorporate them into their residential projects.

Recommended Green Measures for Residential Projects

- Appliances – All kitchen appliances meet Energy Star rating if an Energy Star designation is available for that appliance.
- Reduction in cement uses – Foundation mixes contain at least 20% reduction in cement by replacing cement with products such as flyash, slag, silica fume, or rice hull ash.
- Renewable/Recycled materials – At least 20% of all hard floor covering in the home must be a recycled content material or be of rapidly renewable materials such as bamboo, cork, natural linoleum flooring, engineered wood products, or minimum of 50% recycled content tile.
- Low VOC flooring – At least 80% of the total area of resilient flooring installed must be certified under the Resilient Floor Covering Institute (RFCI) FloorScore program and at least 80% of the total area of carpet meet the CRI Green Label Plus requirements for carpet.
- Radon Resistant construction – Radon resistant construction that meets the criteria of the Basic Techniques for Radon Resistant Construction recommended by the US EPA, is included in the project and the owner has been provided with information on radon risks, the radon resistant construction in the home, and radon testing options.
- Sustainable deck products – All decks surfaces utilize Forest Stewardship Council (FSC) certified wood, salvaged wood, or recycled content plastic or composite decking. Plastic or composite decking materials must be approved by the Office of the State Fire Marshall for use in the Wildland Urban Interface area.
- Low water consumption landscaping – The project does not include lawn area greater than 5% of the lot area and all landscaping is irrigated with a drip irrigation system that includes soil moisture control sensors.
- Kitchen faucet – Maximum flow rate at sink faucets is not greater than 1.5 gallons per minute at 60 psi.

Projects that incorporate these measures will be given priority for plan check, by all City Departments over projects that meet only mandatory requirements. An applicant will be required to submit the checklist along with supporting materials, and a signed testimony that the project complies with the measures with the building permit application. Compliance with the recommended green measures would be verified during the plan check and building inspections. A fine of \$500 would be imposed on projects that, at the time of building occupancy, have not incorporated the green measures but received priority plan check. Funds from the fine would be placed in an account that would be used for City sustainability efforts.

Level 2 – “Priority Plan Check, Allocation, and Recognition”

As part of this program, it is important to utilize the nationally recognized green building certification systems so that the City’s green building success is recognized beyond the City limits. Builders and building owners may also find this important in their marketing efforts. An article in the January 2010 issue of Find Homebuilding magazine emphasized this point when stating that, “builders are looking to certification programs not only because they want to build better homes but also because they want to differentiate themselves from those who aren’t building homes of similar quality.” Certification provides proof that the home has been built to a widely recognized standard. The article goes on to state that, “certification becomes a powerful marketing tool for builders as well as homeowners who plan to sell their home one day.”

Therefore, the second level of voluntary measures requires third party green building certification. Residential buildings that obtain LEED, Energy Star or GreenPoint Rated certification would be eligible for the following:

- Projects would receive priority plan check, over all other projects, by all City Departments.
- Residential projects would have priority on the residential allocation waiting list – 10% of residential allocations would be offered to Green Building projects before other projects on the waiting list.
- Projects would receive recognition at a televised City Council meeting and on the City Website.

When applicants are placed on the Residential Allocation waiting list, they would need to submit a signed testimony that they will pursue green building certification. Procedures for the allocation waiting list and distribution would not be changed, however, 10% of residential allocations received from TRPA each year, would be offered to those pursuing green building certification before being offered to others on the waiting list. Once eligible to receive an allocation, the applicant will need to submit documentation demonstrating their pursuit of the third party certification (i.e., proof of application submittal, contract with a LEED professional, GreenPoint Rater, or Home Energy Rater) prior to receiving a building permit. Proof of final certification will be required prior to issuing a certificate of occupancy. If certification is not obtained when occupancy is required, the applicant may post a security equal to \$10,000 in order to receive a certificate of occupancy for the building. The security would be held until green building certification is obtained. If certification is not obtained within 1 year of occupancy, the security would be forfeited and deposited into the City fund to be used for City sustainability efforts. If the project is not requesting residential allocation(s) they can still qualify for the other incentives and would need to provide a signed testimony and documentation demonstrating their intent to obtain green building certification with their building permit application and provide final certification prior to occupancy.

COMMERCIAL PROJECTS

There is only one level of incentives provided for Commercial projects, “Priority Plan Check, Allocation, and Recognition.”

Buildings that obtain LEED certification or an Energy Star Label would be eligible for the following:

- Projects would receive priority plan check, over all other projects, by all City Departments.
- Residential projects would have eligible for CFA allocation from the City’s unallocated CFA Pool.
- Projects would receive recognition at a televised City Council meeting and on the City Website.

When submitting a Design Review application for a new commercial project, applicants can include a request for Commercial Floor Area (CFA) from the City’s unallocated CFA pool. Currently, these requests would require consideration by the City Planning Commission and eligible projects are required to meet the following criteria:

1. Be within an adopted Community Plan Area,
2. Meet the criteria for CFA allocation required by the applicable Community Plan,
3. Meet all applicable design standards,
4. Include the construction of improvements that will benefit the public and community which are more extensive than required by the applicable Community Plan or City Code, and
5. Provide an economic benefit to the community with enough significance to merit the allocation of the City’s CFA resource.

This policy for allocation of CFA from the unallocated pool was adopted by the City Council on June 7, 2007. In order to provide CFA as an incentive for green building certification, the policy would be amended to include LEED or Energy Star green building certification as one of the criteria. As part of the Design Review application, the applicant would indicate which certification program they will pursue. Prior to issuing a building permit, documentation that demonstrates pursuit of the certification is required (application for Designed to Earn the Energy Star, or LEED Project Registration). Proof of final certification will be required prior to issuing a certificate of occupancy. If certification is not obtained when occupancy is required, the applicant may post a security of \$1.50 per square foot of the building (minimum security of \$10,000) in order to obtain occupancy of the building. The security would be held until green building certification is obtained. If certification is not obtained within 1 year of occupancy, the security would be forfeited and deposited into a City fund to be used for City sustainability efforts. If the project is not requesting commercial floor area they can still qualify for the other incentives.

GREEN BUILDING CERTIFICATION CRITERIA

Green building certification is required to obtain residential level 2 incentives or commercial incentives. Certification programs that have been found to be nationally recognized and achieve green building goals of the City are the LEED, GreenPoint Rated, and Energy Star programs. For more information on these programs, go to:

LEED: <http://www.gbci.org/homepage.aspx>

GreenPoint Rated: <http://www.builditgreen.org/greenpoint-rated>
Energy Star: <http://www.energystar.gov/index.cfm>

Other green building certification programs are gaining recognition and there will likely be new ones in the future. In order to accommodate this trend, there would be an application process for other green building certification programs to be added to the list of LEED, GreenPoint Rated and Energy Star programs that qualify for the level 2 residential incentives and commercial incentives. The application would be considered by the Planning Commission with a recommendation from the Sustainability Commission.

EDUCATION AND OUTREACH

It is important that knowledge of green building within the community in general be increased. Knowledge of green building and its benefits can reduce utility bills, increase the quality of construction, increase indoor quality, and increase comfort of buildings.

At this time, the following efforts would be feasible for City staff to implement within the next year:

- City publication of Residential and Commercial Green Building Guides – Summer 2010
- Hold a workshop on the CalGreen Code mandatory requirements – Fall 2010
- Prepare a list of green building materials and products for local building and hardware stores and encourage them to carry them in their inventories – Fall 2010
- Establish Green Building Information Center at the Services Center building to provide clearinghouse information on Green Building – Continuous Effort
- South Lake Tahoe Green Building Website with information on green building certification programs and local architects, engineers, contractors, etc. with Green Building credentials or experience – Winter 2010
- Advertising at the Services Center for Green Building education and events put on by other organizations